

SECTION 150.00 - SIGNS

SECTION 151.00 - INTRODUCTION

151.01 Purpose. The Idaho Transportation Board has adopted the Manual on Uniform Traffic Control Devices (MUTCD) for use upon highways within the state. The majority of signs used by the Transportation Department are covered in the MUTCD and its supplements. This section is not intended to duplicate the material covered in the MUTCD, but is for the purpose of providing information on signs not covered in the MUTCD and of providing information on specific Department policies for applications on the Idaho state highway system.

Geometric details of standard signs are shown in the publication titled Standard Highway Signs published by the U.S. Department of Transportation/Federal Highway Administration. Details of signs not covered by that publication are available from the Traffic Section by request.

The sign code numbers indicated within this document or on other official Department publications are exclusive for Idaho and do not, in all cases, correspond to those shown in the MUTCD.

151.02 Legal Authority. Traffic signs shall be placed only by the authority of a public body having jurisdiction for the purpose of regulating, warning or guiding traffic. The legal authority for signing Idaho's public highways is established by law under Sections [40-310](#) and [49-202](#), Idaho Code.

The signing for all interstate highways and all rural state highways is the responsibility of the Transportation Department, regardless of any existing cooperative agreement. The responsibility for signing the state highway system in urban areas is shared with the community. Specific responsibilities of each party are outlined in the maintenance agreement with each community. Generally, the state installs and maintains all destination, route and speed signs and the city maintains the other regulatory and warning signing. State and city personnel should work cooperatively in determining signing needs, installation and maintenance requirements and possible enforcement.

No person shall without lawful authority, attempt to or in fact alter, deface, injure, knock down or remove any official traffic control device, including signs or any part thereof. The vandalism of traffic signs is a misdemeanor.

151.03 Sign Placement. Proper sign placement is a function of sign priority, sign position and sign height, and is essential to obtain maximum safety, efficiency and observance. The MUTCD establishes standards; however, some additional criteria is needed to define specific areas.

151.03.01 Priority for Sign Placement. It is recognized that there are situations where two or more signs are needed at approximately the same location or in such close proximity to other signs that the respective messages would not be effective. Regulatory signs should take precedence over other signing. The signing requirements and sign messages should be carefully reviewed to establish the most effective presentation of information for the motorist's needs. Guide signs and miscellaneous information can usually be relocated to a less critical area or the installation totally omitted.

An accepted order of priority for sign types is as follows:

1. Regulatory - speed limit, stop, turn prohibitions
2. Warning - curve, crossroad, width restriction
3. Guide - route trailblazers, destinations
4. Emergency service - hospital, telephone
5. Motorist service - fuel, food, lodging, camping, informational
6. Public transportation - park and ride, bus stop
7. Traffic generator - museums, stadiums, historic buildings
8. General information - times zones, jurisdictional boundaries

151.03.02 Sign Location. The location of a sign is dependent on sign message requirements, motorist visibility needs and roadside safety provisions. As noted in the MUTCD and this traffic manual, the sign type and message dictate in some cases a specific location longitudinally along the highway. Other signs having general messages may be located in the most convenient location for good motorist readability. Although certain signs have specific location requirements the signs can and should be adjusted both longitudinally along the highway and laterally away from the highway to best meet the sign visibility and roadside safety provisions.

The appropriate sequence of considerations to determine a sign's location is as follows:

1. Determine the most appropriate sign placement longitudinally based on sign message, MUTCD guidelines and motorist needs.
2. Review roadway features to determine if the sign can be placed in the vicinity behind a barrier rail, on an existing breakaway illumination pole, roadway structure or roadside area not accessible by vehicles. A fill slope should be avoided because of the post height required to provide sign visibility.

A cut slope is a potential site, provided the area is not reachable by an errant vehicle. However, the sign should not be placed so high on a cut slope that it will be above the driver's cone of vision or headlight coverage.

3. Move the sign laterally away from the edge of the traffic lane to meet the clear zone requirements.
4. If none of the above-listed considerations will satisfy roadside obstacle/clear zone requirements, the sign shall be designed to meet breakaway provisions.

In urban areas, signs generally are not designed with breakaway features because of the danger of flying signs striking pedestrians. Additionally, the roadway curb does partially redirect vehicles, reducing the potential for severe vehicle collisions with roadside signs. However, urban signs should be located to reduce the possibility of vehicle collision. Signs can be placed on light poles or behind sidewalks to reduce the motorist hazard. Note that sign height may have to be increased in order to provide adequate vertical clearance for pedestrians and bicyclists.

The roadside hazard of overhead sign structures can be reduced by placing the signs on roadway structures or installing the sign structure behind a barrier rail. If these options are not available, the sign structure design should include motorist protection, with barrier rail or impact attenuators.

Specific sign placement requirements are as follows:

- Drainage channels should be avoided since the post interferes with water flow and freezing conditions may nullify the post breakaway features.
- An overhead sign should be considered when driver visibility of pavement surface at an off-ramp gore is less than 1,000 feet (300 m). Particular attention should be directed to ramp gore areas located over crest vertical curves.
- Gore exit signs shall be positioned to provide at least a 4 feet (1.2 m) clearance from the edge of the sign to the edge of each roadway shoulder.

Install a cantilever or a full-span structure rather than placing support poles on a narrow, curbed median or in a gore area. If possible, avoid gore locations for sign installations. However, if a support post has to be installed in this area, use a crash attenuating device or breakaway design.

151.03.03 Mounting Height of Signs. The intent of any mounting height is to place the sign generally above any obstruction (such as ground foliage or parked cars), provide headlight illumination, and be within the driver's cone of vision. Minimum mounting heights for visibility requirements as noted in the MUTCD are covered in the Section 2A-18. Minimum mounting heights in construction and maintenance work zones are covered in Section 6F-03.

To promote uniformity, it is suggested that all new sign installations on the state highway system be mounted at least 7 feet (2.1 m) above the highway edge of shoulder, except locations where engineering judgment dictates lowering to the minimum allowed in MUTCD.

For safety reasons, the mounting heights of single post signs located inside the clear zone should be at least 7 feet (2.1 m) from the ground surface to the bottom of the sign and 9 feet (2.7 m) from the ground surface to the top of the sign. This reduces the probability of single post signs and posts going through the windshield or roof of a car when the post is hit. Sign posts located behind guardrail or outside the clear zone do not have to meet these minimum heights for safety.

Multiple post signs should have at least 7 feet (2.1 m) from the ground to the hinge which is located about 3 inches (75 mm) below the bottom of the sign.

151.04 Reflectorization. All signs on the state and federal-aid highway systems shall be reflectorized.

Engineering-grade retroreflective sheeting should be used for most signs. Signs installed by contract shall use high intensity (Type III) sheeting for Stop, Wrong Way, Do Not Enter, and Yield signs. Orange background work zone signs shall use a minimum high intensity (Type III) sheeting for special emphasis and increased target value. All overhead sign legends shall have high-intensity legends or shall be illuminated.

All interstate guide and directional signs should have engineering-grade retroreflective sheeting background. The legends shall be high-intensity (Type III) sheeting.

151.05 Posts and Mountings. All ground-mounted signs located in the clear zone are fixed object hazards and should be designed as safe as feasible. If they cannot be relocated to a point where they are less likely to be struck, the impact severity should be reduced by using an approved breakaway design.

Normally, all permanent sign posts on interstate projects should be steel, with a breakaway design meeting the placement requirements noted above. Unpainted, breakaway wood posts may be used on federal-aid primary and secondary projects; however, steel posts are preferred in urban areas.

A portable or removable type of mounting is desirable for signs used temporarily or intermittently. Refer to MUTCD Section 6G-02 for duration and mounting specifications.

Wood signposts and the back of plywood sign panels do not need to be painted. Do not paint the backside of aluminum signs except when needed as a glare deterrent.

Install all new posts, whether for new installations or maintenance replacement, according to current standards.

151.05.01 General. Signs less than 8 feet (2.4 m) wide may be mounted on a single post. Signs 2.4 m (8 feet) wide or wider shall be mounted on two posts.

In accordance with the AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals, ITD uses design wind loads of 70 mph for wood sign posts and 80 mph for steel sign posts. Since these wind loads are constant for all signs throughout the state, the tables for posts and hinges in this section have been simplified to give the “Allowable Sign Load” in terms of the total area of signs multiplied by the height of the centroid of the total sign area.

151.05.02 Single Sign Post Installations. Types “B”, “D”, and “E” sign post may be used for single post installations. To determine the size of post needed, first calculate the sign load on the base of the post by multiplying the area of sign face in square meters (feet) times the height in feet (meters) from the base of the post to the centroid of the sign face. Refer to Figures [151.05.02-01](#) and [151.05.02-02](#).

Signs that are 30 inches (762 mm) in width or less may be bolted directly to the post. Flat signs that are over 30 inches (762 mm) in width should be back braced to minimize wind vibration and damage caused when hit with snow or slush from plowing operations.

Extruded aluminum signs mounted on a single post require an angle iron bolted on each side of the post as shown in Standard Drawing I-8-E for Type D wood posts. These same angle irons may be used on Type B steel posts.

Post Type	Nominal Post Size Inches (mm)	Weight Lb/Ft (kg/m)	Approved Breakaway Device	Found Type	Section Modulus In^3 (10^3 mm^3)	Depth Of Section Inches (mm)	Allowable Sign Load SFxFt^2 ($\text{m}^2 \times \text{m}$)
B-2	3x4 (76 x 102)	8.15 (12.1)	AS4-LP	A	2.62 (42.9)	4 (102)	330 (9.35)
B-3	4 x 6 (102 x 152)	11.97 (17.8)	AS4-LP	A	5.81 (95.1)	6 (152)	560 (15.87) *
B-4	4 x 8 (102 x 203)	14.53 (21.6)	AS4-LP	A	8.83 (145)	8 (203)	675 (19.13) *
E-1	2x2 (51 x 51)	2.42 (3.6)	E-1	E-1	0.37 (6.06)	2 (51)	45 (1.27)
E-2	2½ x 2½ (64 x 64)	4.01 (5.96)	E-2	E-2	0.78 (12.78)	2.5 (64)	96 (2.72)
* Limited by strength of base							
Post Type	Nominal Post Size Inches (mm)	Weight Lb/Ft (kg/m)	Approved Breakaway Device	Buried Depth Feet (m)	Section Modulus In^3 (10^3 mm^3)	Depth Of Section Inches (mm)	Allowable Sign Load SFxFt^2 ($\text{m}^2 \times \text{m}$)
D-1	4 x 4 (100 x 100)	3 (4.5) ±	Undrilled	3.5 (1.07)	7.15 (117)	3½ (89)	60 (1.70)
D-2	4 x 6 (100 x 150)	5 (7.5) ±	1½" (38 mm) Hole	4.0 (1.22)	17.29 (283)	5½ (140)	121 (3.43)
D-3	6 x 6 (150 x 150)	8 (12) ±	2" (50 mm) Hole	5.0 (1.52)	26.40 (433)	5½ (140)	185 (5.24)
D-4	6 x 8 (150 x 200)	11 (16.4)±	3" (75 mm) Hole	6.0 (1.83)	48.26 (791)	7½ (190)	338 (9.58)
D-M	8 x 8 (200 x 200)	11 (16.4)	2 - 1 7/8" (48 mm)	6.0 (1.83)	63.70 (1044)	7 7/8 (200)	743 (21.06)
D-L	8 x 15 (200 x 375)	17 (25.3)	Wide Saw Cuts 1 - 8 7/8" (225 mm) Wide Saw Cut & 1 - 4 7/8" (124 mm) Wide Saw Cut	6.0 (1.83)	176.00 (2884)	14 7/8 (378)	2,053 (58.18)

*ALLOWABLE SIGN LOAD: The total area in square feet (m^2) of all signs on the post multiplied by the distance in feet (meters) from the base of the post at the top of its foundation to the centroid of the total sign area.

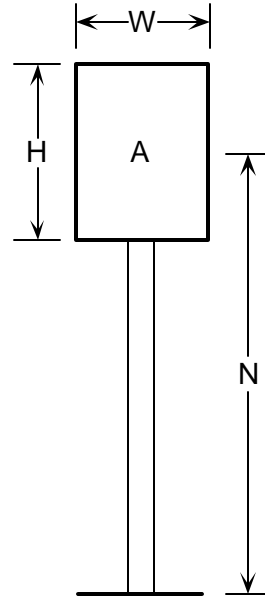
Figure 151.05.02-01 Allowable Sign Loads* On Approved Single Posts

SINGLE POST WITH SINGLE SIGN

$$L = A \times N$$

(SqFt x Ft) (SqFt) (Ft)

L = Sign load on sign post base (SqFt x Ft)
A = Area of sign face in square feet
W = Overall width of sign face in feet
H = Overall height of sign face in feet
N = Distance in feet from top of foundation to sign centroid.



SINGLE POST WITH TWO OR MORE SIGNS

$$L = A1N1 + A2N2$$

$$A = A1 + A2$$

$$N = L/A$$

A1, A2, etc = Area of sign face(s) in square feet
N1, N2, etc = Distance in feet from top of foundation to the centroid of each respective sign

L = Sign load on sign post base (SqFt x Ft)
A = Area of sign face in square feet
W = Overall width of sign face in feet
H = Overall height of sign face in feet
N = Distance in feet from top of foundation to sign centroid.

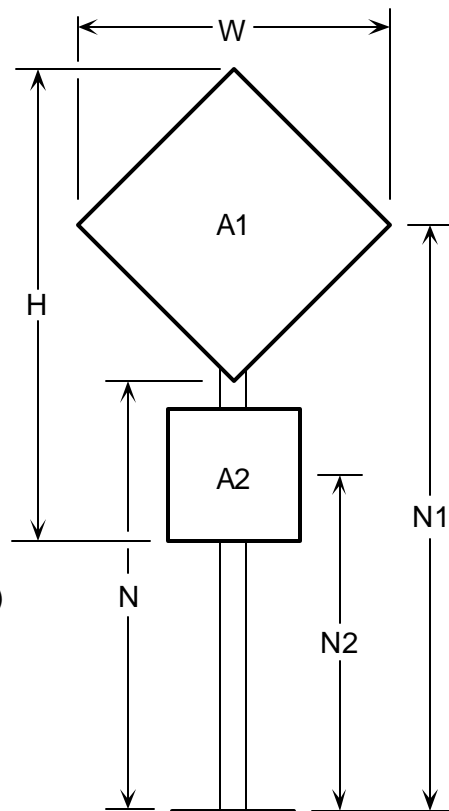


Figure 151.05.02-02

Calculation Of Sign Load On Post Base For Single Sign Post Installations

151.05.03 Multiple Sign Post Installations. Generally, two posts are only used on signs having a width of 8 feet (2.4 m) or more because it is desirable to have a 7-foot (2.1 m) minimum clearance between the posts so a car will normally not hit more than one post. The smaller posts, A-1, A-2, D-1, and D-2, may have less than the 7-foot (2.1 m) clearance if necessary.

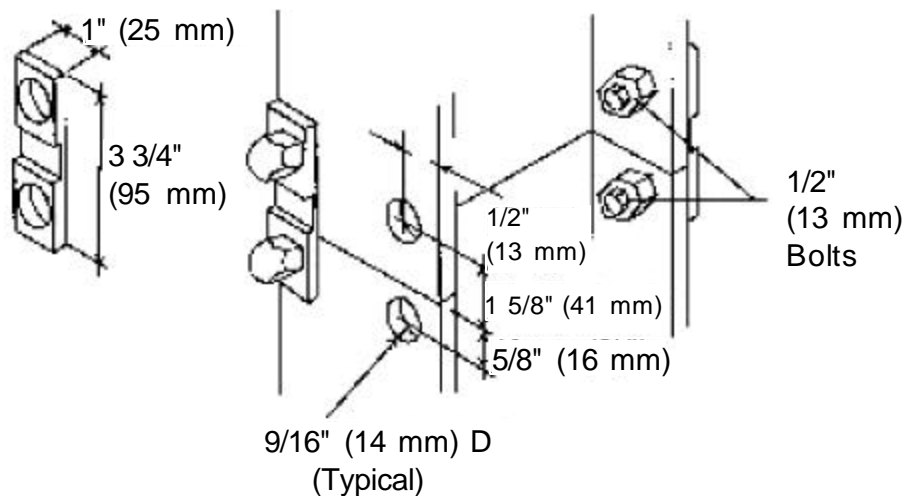
Unless located behind guardrail or outside the clear zone, all two-post installations should have an approved breakaway design at the base and an approved hinge or weakened joint just below the sign. The hinge should be at least 7 feet (2.1 m) above the ground surface for safety so the post can swing up and clear an impacting car. The mounting height of the signs should also comply with the minimums in MUTCD Section 2A-18. See Figures [151.05.03-01](#) and [151.05.03-02](#) for allowed sign loads per post.

Extruded aluminum signs will require angle irons bolted to each side of the posts when installed on wooden posts (Type D). See Standard Drawing I-8-E.

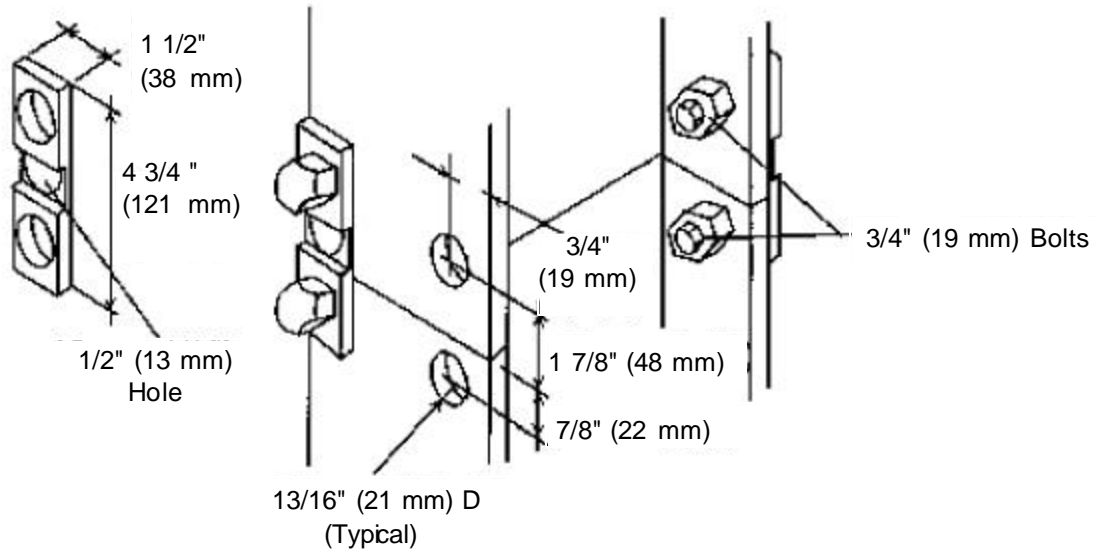
All brackets, brace angles, clips, bolts, and other hardware needed to mount the sign face on the posts shall be included in the cost of the bid item "Breakaway Sign Post Installation Type____" for Type A posts and included in the bid item for Type D wood posts.

Once a post size is selected, the hinge should be checked (see Figure [151.05.03-02](#)) to see if the moment load above the hinge is within the allowable limits for the standard hinge for that size post.

Three types of hinge plate sets are approved as shown on the following pages.

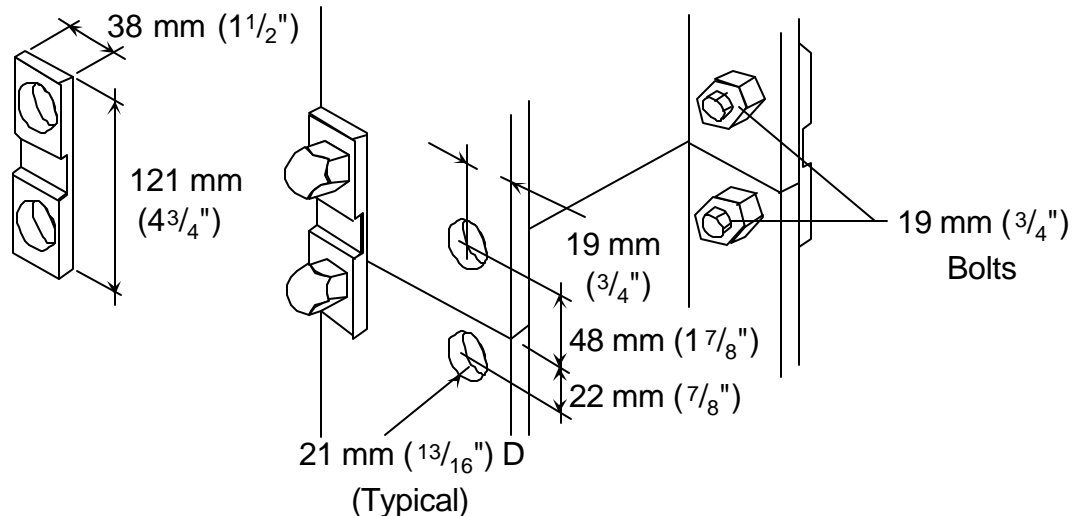


The SBHA1B hinge is 1 inch (25 mm) by 3¾ inches (95 mm) with ½ inch (13 mm) bolts. It comes standard with the AI6-LP Breaksafe Base kit for A-1 posts. If a sign installation exceeds the allowable design load on this hinge, an A-2 post should be used.



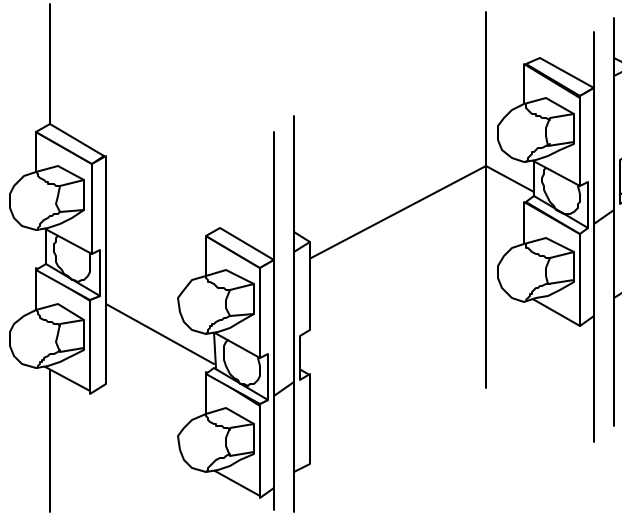
SBHB1B

The SBHB1B hinge is 1½ inches (38 mm) by 4¾ inches (121 mm) with a ½ inches (13 mm) hole in the center and uses ¾ inches (10 mm) bolts. It comes standard with the B-525-LP Breaksafe Base kit that is used on A-2, A-3 and A-4 posts. If a sign installation exceeds the allowable design load on this hinge, SBHB2B hinge plates may be substituted for a 40% increase in the allowable design load. The SBHB2B hinge plate is identical to the SBHB1B plate except it doesn't have the ½ inches (13 mm) hole.



SBHB2B

The SBHB2B hinge is 1½ inches (38 mm) by 4¾ inches (121 mm) and uses ¾ inches (19 mm) bolts. It comes standard with the B-650-LP Breaksafe Base kit that is used on A-8 and A-9 posts. If extra strength is needed, two SBHB1B hinge sets may be used with hinge plates both inside and outside the flanges on the posts as shown below.



DOUBLE SBHB1B

Post Type	Nominal Post Size Inches (mm)	Weight Lb/Ft (kg/m)	Approved Breakaway Device	Found Type	Section Modulus In^3 (10^3 mm^3)	Depth Of Section Inches (mm)	Allowable Sign Load SF x Ft ($\text{m}^2 \times \text{m}$)
A-1	W6 x 9 (W150 x 14)	9 (14)	AI4-LP	A	5.56 (91)	5.9 (150)	701 (19.87)
A-2	W8 x 10 (W200 x 15)	10 (15)	B-525-LP	B	7.81 (128)	7.9 (200)	853 (24.17)
A-3	W8 x 13 (W200 x 19)	13 (19)	B-525-LP	B	9.91 (163)	8 (203)	1,249 (35.39)
A-4	W8 x 18 (W200 x 27)	18 (27)	B-525-LP	B	15.2 (249)	8.1 (207)	1,915 (54.27)
A-8	W12 x 14 (W310 x 21)	14 (21)	B-650-LP	C	14.9 (244)	11.9 (303)	1,627 (46.11)
A-9	W14 x 22 (W360 x 33)	22 (33)	B-650-LP	C	29.0 (475)	13.7 (349)	2,730 (77.36) *
* Limited by strength of base							
Post Type	Nominal Post Size Inches (mm)	Weight Lb/Ft (kg/m)	Approved Breakaway Device	Buried Depth Feet (m)	Section Modulus In^3 (10^3 mm^3)	Depth Of Section Inches (mm)	Allowable Sign Load SFxFt ($\text{m}^2 \times \text{m}$)
D-1	4 x 4 (100 x 100)	3 (4.5) ±	Undrilled	3.5 (1.07)	7.15 (117)	3½ (89)	60 (1.70)
D-2	4 x 6 (100 x 150)	5 (7.5) ±	1 ½ " (38 mm) Hole	4.0 (1.22)	17.29 (283)	5½ (140)	121 (3.43)
D-3	6 x 6 (150 x 150)	8 (12) ±	2" (50 mm) Hole	5.0 (1.52)	26.40 (433)	5½ (140)	185 (5.24)
D-4	6 x 8 (150 x 200)	11 (16.4) ±	3" (75 mm) Hole	6.0 (1.83)	48.26 (791)	7½ (190)	338 (9.58)
D-M	8 x 8 (200 x 200)	11 (16.4)	2 - 1 7/8" (48 mm) Wide Saw Cuts	6.0 (1.83)	63.70 (1044)	7 7/8 (200)	743 (21.06)
D-L	8 x 15 (200 x 375)	17 (25.3)	1 - 8 7/8" (225 mm) Wide Saw Cut & 1 - 4 7/8" (124 mm) Wide Saw Cut	6.0 (1.83)	176.00 (2884)	14 7/8 (378)	2,053 (58.18)

*ALLOWABLE SIGN LOAD: The total area in square feet (m^2) of all signs multiplied by the distance in meters (feet) from the base of the longest post at the top of its foundation to the centroid of the total sign area, divided by the number of posts.

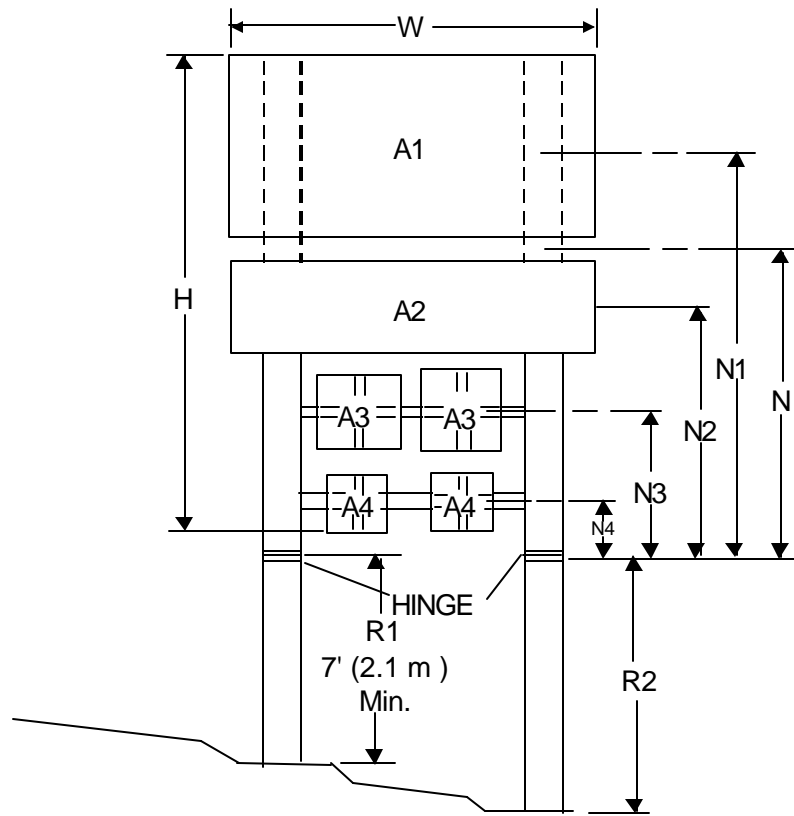
Figure 151.05.03-01 Allowable Sign Load* (Per Post) For Posts Approved For Multiple Sign Post Installations

POST TYPE	POST SIZE INCHES (MM)	OPTIONAL LOWER STRENGTH HINGE SET	STANDARD HINGE SET	OPTIONAL EXTRA STRENGTH HINGE SET
A-1	W6 x 9 (W150 x 14)		SBHA1B 168 (4.76)	
A-2	W8 x 10 (W200 x 15)		SBHB1B 386 (10.94)	SBHB2B 547 (15.50)
A-3	W8 x 13 (W200 x 19)			
A-4	W8 x 18 (W200 x 27)			
A-8	W12 x 14 (W310 x 21)	SBHB1B 579 (16.41)	SBHB2B 820 (23.24)	DOUBLE SBHB1B 1146 (32.48)
A-9	W14 x 22 (W360 x 33)	SBHB1B 675 (19.13)	SBHB2B 958 (27.15)	DOUBLE SBHB1B 1338 (37.92)

*SIGN LOAD ON HINGE: The total sign area in square feet (m²) multiplied by the distance from the hinge to the centroid of the total sign area in feet (meters), divided by the number of sign posts.

See Figure [151.05.03-03](#) for the method of calculating the sign load on the post hinge.

Figure 151.05.03-02 Allowable Sign Load* On Breaksafe Hinge Plates



$$L_{\text{HINGE}} = \frac{2(A_4)(N_4) + 2(A_3)(N_3) + (A_2)(N_2) + (A_1)(N_1)}{(\text{Number of Posts})}$$

$$N = \frac{L_{\text{HINGE}}}{A} \times \text{Number of Posts}$$

$$L_{\text{BASE}} = \frac{N + R_2}{N} \times L_{\text{HINGE}} = L_{\text{HINGE}} + \frac{R_2(A)}{(\text{Number of Posts})}$$

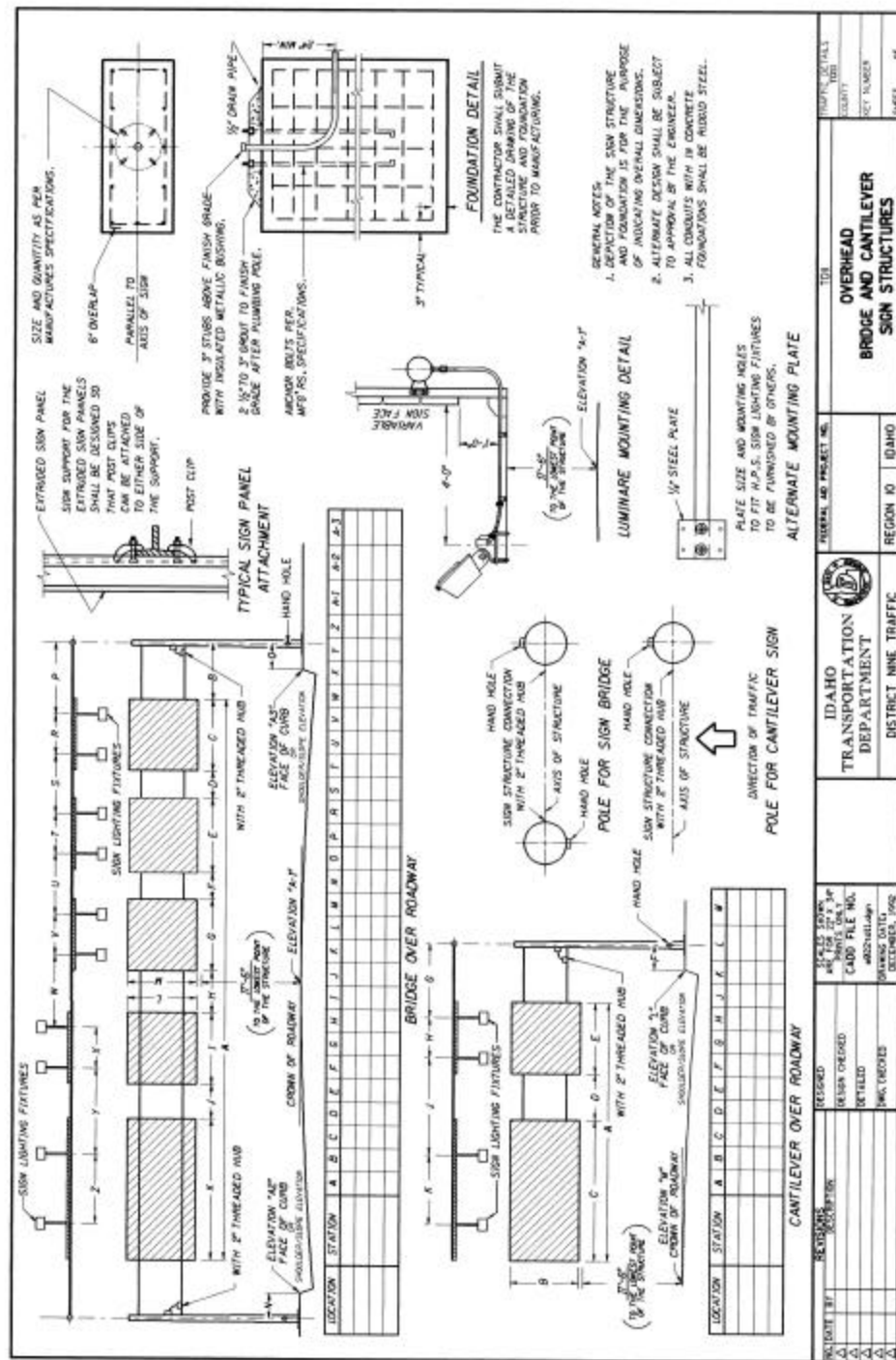
- A1,A2,A3,A4,etc. = Area of sign in square feet (m²)
- A = Total area of all signs
- N1,N2,N3,N4,etc = Distance in feet (meters) from center of hinge to center of each respective sign
- N = Distance in feet (meters) from center of hinge to centroid of total area of all signs
- R1,R2 = Distance in feet (meters) from base of each post to center of hinge
- L_{HINGE} = Sign load on the hinge per post SqFt x Ft (m² x m)
- L_{BASE} = Sign load at base of post SqFt x Ft (m² x m)
(Use longest post to determine post size)
- H = Overall height of sign faces in feet (meters)
- W = Overall width of sign faces in feet (meters)

Figure 151.05.03-03 Calculation Of Sign Load On Multiple Post Installations

151.05.05 Overhead Sign Structures and Foundations. Requests for overhead sign structure foundation designs shall be coordinated through the Materials Section. The necessary data such as sign sizes, location of signs on the structure, elevations and number of sign lights shall be submitted with requests.

A standard plan for overhead sign structures for conventional designs is illustrated in Figure [151.05.05-01](#).

Requests for special overhead sign structure designs shall be coordinated through the Bridge Section.



151.06 Maintenance. At least annually conduct a daytime and a nighttime inspection to ensure that all signs, supports and locations are adequate. In some areas, make the inspection twice a year or more, depending on the environment. Sometimes corrosive atmospheric conditions can be caused by chemical plants and factories that may have a tendency to obscure or damage retroreflective sheeting and sign faces. Inspections should be documented through a dated record of work performed or a diary entry.

It is important to maintain a program of washing and cleaning sign faces particularly after the winter months or at any other time when signs are adversely affected by a dust or mud problem.

Refer to the Maintenance Manual, Section 321.0, for additional guidance.

151.07 Sign Inventory and Maintenance Reporting. The Department has an on-line inventory and work status report for each sign installed on the State Highway System. This report contains information on each type of sign materials, placements, type of mountings, work activity and dates of work on the sign installation. It is a direct, on-line database that allows the districts to enter and retrieve data pertinent to their signing activities. The districts can use either the Sign Maintenance Report (Figure [151.07-01](#)) or Sign Maintenance Field Report (Figure [151.07-02](#)) to facilitate reporting of field maintenance activities.

Individual sign inventory data entries should be made for all signs on the State highway system as well as all off-systems signs that are directly related to the State highway system, such as “Stop Ahead” (Section [155.01](#)) and Low Clearance (Section [167.05](#)) signs. In addition all placards, such as route markers, camera and binocular signs, mounted directly to another sign should be inventoried as separate inventory data entries.

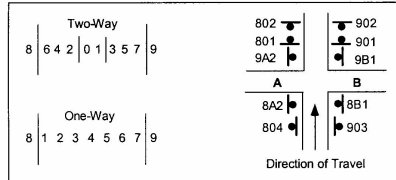
The Idaho Transportation Department does not strictly adhere to the MUTCD sign numbering sequence, and special attention should be given when placing sign numbers on traffic plans and sign orders. Contained within the Sign Chart is a cross-reference chart to MUTCD sign numbers, where applicable. Any questions on the traffic sign system or traffic sign inventory should be directed to the Headquarters Traffic Section.

Sign Maintenance Report



Key #: _____ Installation Date: ____/____/____ Removal Date: ____/____/____
Year Dist Sequence # Time: ____:____ AM/PM Time: ____:____ AM/PM
Segment: _____ Milepost: _____ Route: _____ IC No: _____
Assy No: _____ Speed: _____
Sign No: _____ Size: W ____ x H ____ Var Size: W ____ x H ____
Catalog No: _____ Legend: _____

Location: _____
Place Lane Traffic Served
___ B Overhead Grade Separator/Bridge
___ C Overhead Sign Cantilever
___ D Building
___ G Ground
___ K Kiosk
___ M Signal Mast Arm
___ P Bridge Pier/Column/Abutment
___ R Bridge/Guard Rail
___ S Overhead Sign Bridge



Blank Material: ____ Color: ____ Legend/Background: ____
___ 01 Aluminum ___ 00 Black-White ___ 09 Red-Black-White ___ 00 Engr Grade / Engr Grade
___ 02 Extruded Aluminum Panel ___ 01 Red-White ___ 10 Red-Black-Yellow ___ 01 Rem (buttons) / Nonreflective
___ 04 Plywood ___ 02 Green-White ___ 11 Red ___ 02 Rem Hi-Int / Nonreflective
___ 05 Aluminum over Extrusions ___ 03 Blue-White ___ 12 Green ___ 03 Rem (Buttons) / Reflective
___ 99 See Remarks ___ 04 Brown-White ___ 13 Yellow ___ 04 Rem Hi-Int / Reflective
___ 05 Red-White-Blue ___ 14 Black-Red ___ 05 Vinyl Sheeting Nonrefl/ Refl
___ 06 Black-Orange ___ 19 Black-Fl. Yellow ___ 06 Vinyl Sheeting Refl / Nonrefl
___ 07 Black-Yellow ___ 99 See Remarks ___ 07 Dir-Applied (Hi-Int) Refl/ Refl
___ 08 Blue-Yellow ___ 08 Screen Ink Nonrefl/ Refl
___ 09 Nonrefl / Hi-Int Refl
___ 10 Hi-Int / Hi-Int Refl
___ 11 Electronic
___ 99 See Remarks

Agreement: ____ Utilities: ____ Miscellaneous: ____
Fabrication / Installation / Maintenance Costs
___ 1 ITD ___ 00 None ___ 10 Hinged ___ 15 Var Msg Bd (wired)
___ 2 Federal Agency ___ 01 Underground ___ 11 Illuminated (wired/steady) ___ 16 Var Msg Bd (solar)
___ 3 State Agency (not ITD) ___ 02 Overhead ___ 12 Illuminated (solar/steady) ___ 99 See Remarks
___ 4 County ___ 03 Both ___ 13 Illuminated (wired/flash)
___ 5 City ___ 99 See Remarks ___ 14 Illuminated (solar/flash)
___ 6 Local Highway District
___ 7 Private

Sign Work Date: ____/____/____ Time: ____:____ AM/PM Retroreflectivity: ____

Sign Work: (Select 1 or 2)
___ 00 Removed Sign ___ 05 Renewed Face ___ 10 Bracket, Brace Mtce ___ 15 Electronic Repair
___ 01 New Installation ___ 06 Rebolted to Post ___ 11 Cleaned Face ___ 16 Retro Reading
___ 02 Patched Sign ___ 07 Relocated Sign ___ 12 Straightened Sign ___ 17 Changed Sign Number
___ 03 Replaced Sign ___ 08 Replaced Buttons ___ 13 Folded/Covered Sign ___ 18 Added Sign Data
___ 04 Adjusted Height ___ 09 Overlayed Sign ___ 14 Unfolded/Uncovered Sign ___ 99 See Remarks

Post Work Date: ____/____/____ Time: ____:____ AM/PM

Post Work: (Select 1 or 2) Post Type: ____ No. Posts: ____ Length 1: ____ Length 2: ____
___ 00 Removed Post ___ 00 Stil/Alum channel/U Non-Bkwy ___ 13 Stil Rect Bkwy (Kick) ___ 25 Breaksafe H-Beam
___ 01 Replaced Post ___ 01 Stil Round Non-Bkwy ___ 14 Wood 4x4 Drilled ___ 26 Breaksafe Tubular Steel
___ 02 Reset Post ___ 02 Stil H-Beam Non-Bkwy ___ 15 Wood 4x6 Drilled ___ 27 Signal Pole
___ 03 Straightened Post ___ 04 Wood 4x4 Undrilled ___ 16 Wood 6x6 Drilled ___ 28 Overhead Sign-Bridge
___ 04 Rebolted to Base ___ 05 Wood 4x6 Undrilled ___ 17 Wood Round Bkwy ___ 29 Bridge/Guard Rail
___ 05 Drilled Breakaway ___ 06 Wood 6x6 Undrilled ___ 18 No Post - Br/Gr Sep ___ 30 No Post - Bldg Mounted
___ 06 Moved Off Road ___ 07 Wood Round Non-Bkwy ___ 19 Wood 6x8 Drilled ___ 31 Tubular Steel Non-Bkwy
___ 07 Replaced Foundation ___ 08 Light Pole ___ 21 Signal Mast Arm ___ 32 R/W Fence
___ 08 Adjusted Height ___ 09 Utility Pole ___ 22 Tubular Stil Slip Base ___ 33 No Post - Kiosk Mounted
___ 09 Added Post ___ 10 Stil Channel/U Bkwy ___ 23 Unistrut / Telespar ___ 50 No Post - Part of Assembly
___ 10 Bracket Mtce ___ 11 Stil Round Bkwy ___ 24 Wood 6x6 Undrilled ___ 99 See Remarks
___ 50 Added Post Data ___ 12 Stil H-Beam Bkwy
___ 99 See Remarks

Reason Work Performed: Sign (S) / Post (P)

S	P	S	P	S	P
___ 00 Removed	___ 14 Delamination	___ 28 Old, Rotten			
___ 01 Projectile	___ 15 Wind Damage	___ 29 Political Request			
___ 02 Paint	___ 16 Split, Broken	___ 30 Logo, TODS Contract			
___ 03 Shot	___ 17 Corroded Files	___ 31 Curve Data Update			
___ 04 Stolen	___ 18 New Sign Required	___ 32 Animal Damage			
___ 05 Legend Change	___ 19 Safety Improvement	___ 33 Road Realignment			
___ 06 Size Change	___ 20 Slide Damage	___ 34 Road Resurface			
___ 07 MUTCD Conformance	___ 21 Color Change	___ 35 Engineer Request			
___ 08 Flood Damage	___ 22 Vandalism	___ 36 Adopt-a-Hwy			
___ 09 Motor Vehicle Damage	___ 23 Fire, Lightning	___ 37 Sign Inventory Change			
___ 10 Snow Removal Damage	___ 24 Detour	___ 50 Test Section			
___ 11 Mtce Agreement	___ 25 Route Change	___ 99 See Remarks			
___ 12 Construction	___ 26 Minute Entry				
___ 13 Reflectivity Loss, Discolored	___ 27 Seasonal				

Figure 151.07-01 Sign Maintenance Report (ITD-2668)



Sign Maintenance Field Report

[illegible]

Figure 151.07-02

Sign Maintenance Field Report (ITD-2684)

151.08 Sign Requests and Appeals. All requests for highway signing by an individual, organization, or governmental agency shall be directed in writing to the District Traffic Engineer. The District shall submit to the State Traffic Engineer for further action:

- all sign requests that conflict with the provisions of this manual,
- all sign requests that are not specifically addressed in this manual, and
- all sign requests that have been denied by the District.

Submittal of sign requests or appeals to the State Traffic Engineer shall include all documentation from the original sign request, any additional information gathered by the District, and a letter addressing the District's recommendations. This information shall be submitted within 30 days of the original request or denial. Any request or appeal submitted without complete documentation will be returned to the District Traffic Engineer.

The State Traffic Engineer shall have 10 days to make a written response to all sign requests or appeals, either approving the request or concurring with the District's denial. If signing is approved, the conditions of payment, manufacture, placement, and maintenance shall be addressed in the State Traffic Engineer's response. If the State Traffic Engineer concurs with the District's denial, the request shall be submitted to the Chief Engineer for FINAL APPEAL.

When an appeal is submitted to the Chief Engineer, an additional 10 days shall be allowed for a final decision. The Chief Engineer's decision shall be considered FINAL. This decision shall be stated in a letter directed to the District Traffic Engineer and the individual, organization, or governmental agency that submitted the original sign request. If the Chief Engineer approves the request, the letter will address the conditions of payment, manufacture, placement, and maintenance of the signing.

Signing types not addressed by this policy include:

- Tourist Oriented Directional Signing (TODS) on primary and secondary highways,
- Logo signing on the interstate and other fully controlled access highways, and
- all outdoor advertising signs visible from the State highway system (both on-premise and off-premise).

The above-mentioned signs have separate signing policies. Requests for these signs should be directed to the individual in the appropriate District that administers these signing programs.

151.09 Miscellaneous Sign Forms. The sign form used in sign designs and plan preparation is shown in Figures [151.09-01](#). The sign order form is shown in Figure [151.09-02](#).

[illegible]

Figure 151.09-01 Signing Erection

Date: _____

[illegible]

W/A	Rule	Function	Activity	Org
-----	------	----------	----------	-----

Requested by	Date
District Approval	Date
HQ Traffic Approval	Date

Figure 151.09-02 Highway Sign Order Form (ITD-1671)

151.10 Miscellaneous Traffic Detail Sheets. Traffic Section detail drawings used in plan preparation as a supplement to Idaho Transportation Department Standard Drawings are shown in Figures 151.10-01 through 151.10-08. These traffic detail sheets may be obtained from the Headquarters Traffic Section.

NOTES:

- 1.1 FOR GENERAL NOTES SEE SHEET NO. _____
- 1.2 FOR ARROW DETAILS SEE SHEET NO. _____
- 1.3 FOR ARROW DETAILS SEE STANDARD DRAWING 1-14.
- 1.4 ALL SIGNS ON THIS SHEET SHALL HAVE GREEN REPLICATED BACKGROUNDS.
- 1.5 ALL SIGNS SHALL HAVE CORNER RADIUS OF 1/4" MINUS CORNERS.
- 1.6 SIGNS AS SHOWN ARE FOR RIGHT EXIT.
- 1.7 LEFT EXITS SHALL HAVE EXIT PANEL MOUNTED TO LEFT SIDE OF SIGN.

INTERCHANGE ADVANCE GUIDE SIGNS

REVISIONS		DESIGNER		CHECKED		APPROVED		PROJECT NO.		DISTRICT		SHEET	
NO.	DATE	BY	DESCRIPTION	BY	DATE	BY	DATE	BY	BY	BY	BY	BY	BY

INTERSTATE GUIDE SIGNS

REVISIONS		DESIGNER		CHECKED		APPROVED		PROJECT NO.		DISTRICT		SHEET	
NO.	DATE	BY	DESCRIPTION	BY	DATE	BY	DATE	BY	BY	BY	BY	BY	BY

Figure 151.10-01 Interstate Guide Signs

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

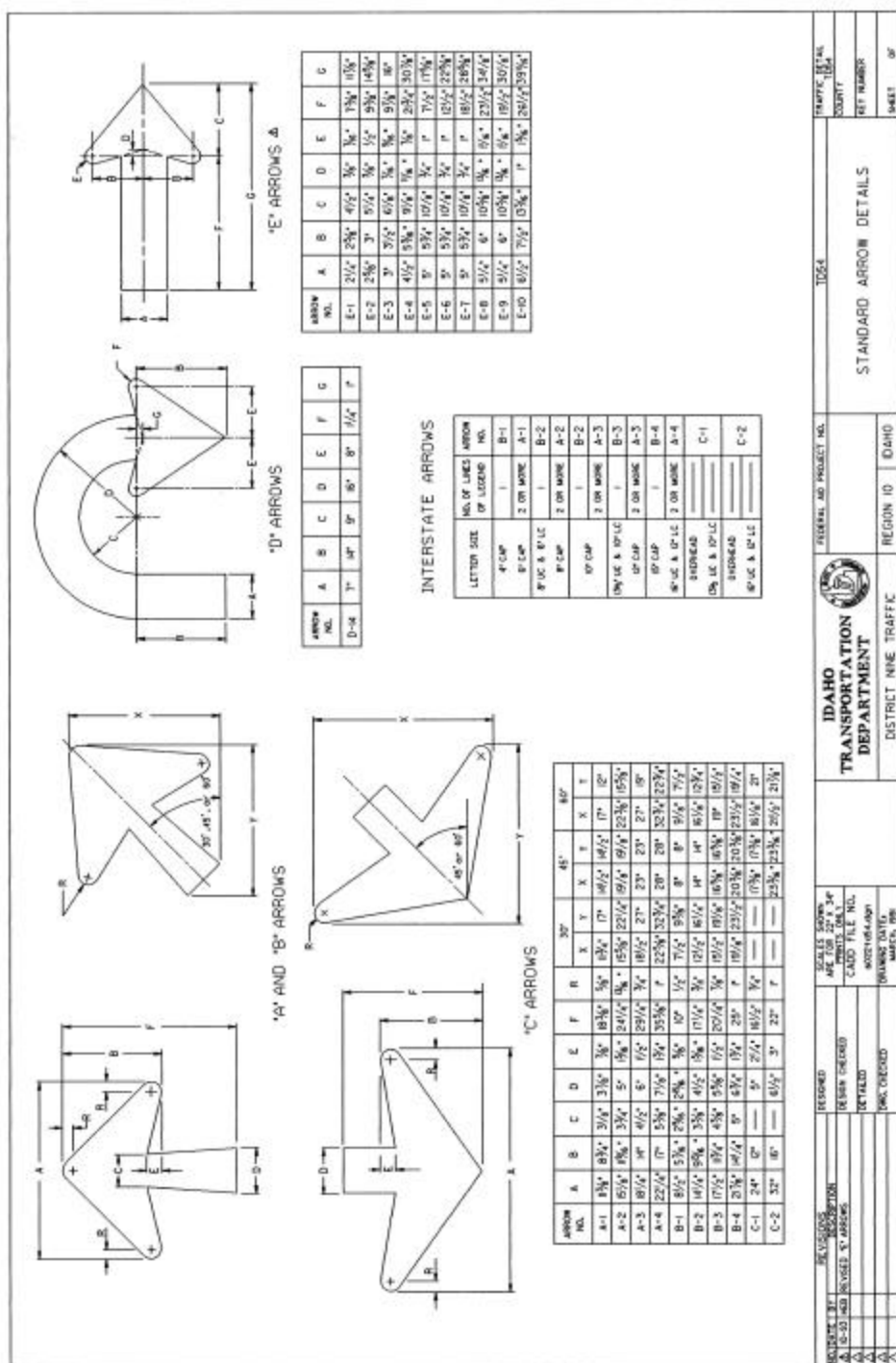
TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E

TYPE	SIZE	NAME	A	B	C	D	E



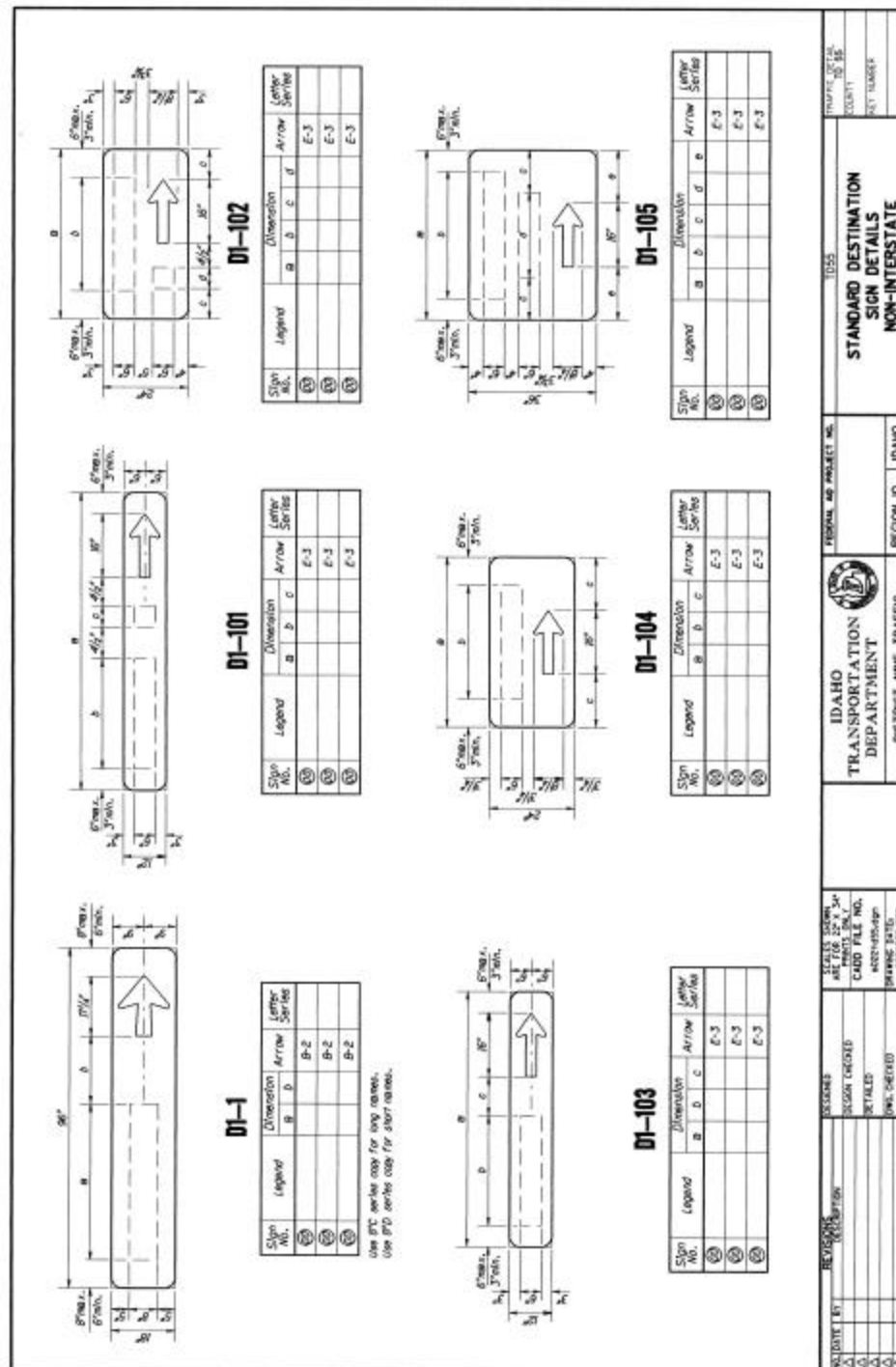
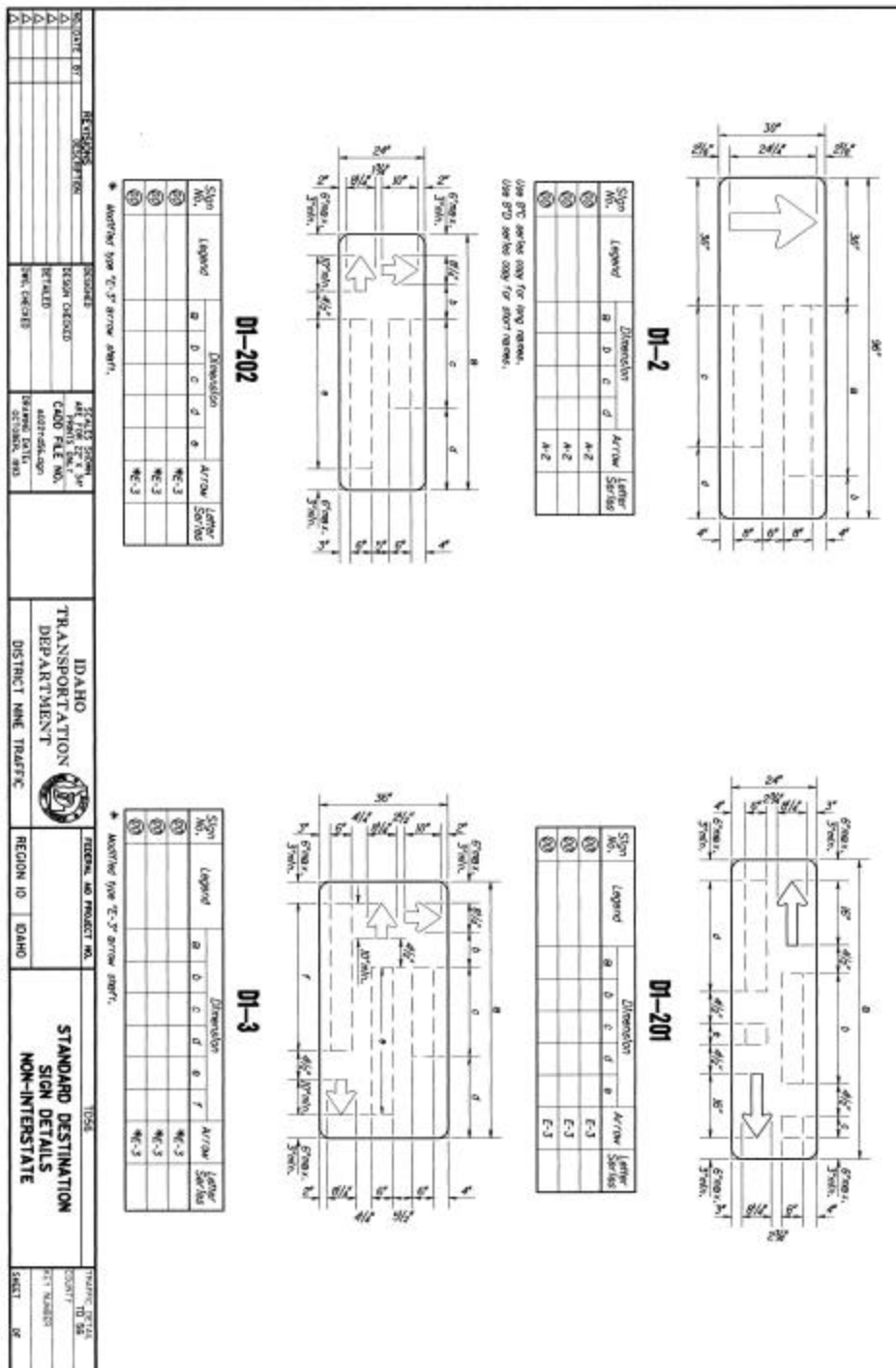


Figure 151.10-06 Standard Destination Sign Details Non-Interstate



D2-1

Sign No.	Legend	Dimension			Letter Series
		a	b	c	
1					
2					
3					

Use 6" series cap for long names.
Use 6" series cap for short names.

D2-2

Sign No.	Legend	Dimension						Letter Series
		a	b	c	d	e	f	
1								
2								
3								

Use 6" series cap for long names.
Use 6" series cap for short names.

D2-3

Sign No.	Legend	Dimension							Letter Series
		a	b	c	d	e	f	g	
1									
2									
3									

Use 6" series cap for long names.
Use 6" series cap for short names.

IN THE WIDTH OF THIS SIGN MAY BE INCREASED IN 6" INCREMENTS FOR LONG DESTINATION NAMES.

REQUESTED BY PROJECT NO. SHEET NO.	RECORD DESIGN CHECKED DETAIL CHECKED SPEC. CHECKED	PROJECT FILE NO. CADD FILE NO. ESTIMATE NO. CONTRACT NO.	IDAHO TRANSPORTATION DEPARTMENT DISTRICT NINE TRAFFIC	REGION ID IDAHO	FEDERAL AID PROJECT NO.	TEST	STANDARD DESTINATION SIGN DETAILS NON-INTERSTATE	DRAWING NO. SHEET NO. SHEET NO. SHEET NO.
						SHEET NO.	SHEET NO.	

Figure 151.10-08 Standard Destination Sign Details Non-Interstate

SECTION 152.00 - FOREST SERVICE SIGNS

152.01 General. The Transportation Department Memorandum of Understanding (MOU) with Forest Service regions broadly assigns full signing responsibilities to the state on highways in the national forests. A Memorandum of Understanding has been signed with the U.S. Forest Service for regions 1, 3 and 4, listing signing procedures on highway right-of-way through forest land. The state is responsible for the following signing:

Interstate highway system, including interchange area - all signing

- State highway system:
- Warning and regulatory signs
 - o Pedestrian control devices
 - o Construction and maintenance signing for district or contractor activities
 - o Route markers, destination and mileage signs

The responsibility for miscellaneous guide signing is less specific. The MOU provides the District Engineer the opportunity to coordinate signing with the Forest Service and, with the Forest Service's concurrence, assume responsibility for most of the miscellaneous guide signs. In any case, all signing shall correspond to the requirements of the MUTCD.

The signing on federal land other than that of the U.S. Forest Service shall follow the MUTCD and the general practice used in national forests.

152.02 Signing Maintained By the Idaho Transportation Department. All regulatory and warning signs shall comply with the MUTCD and be installed by the Department. Guide signing to be maintained by the Departments with concurrence of the U.S. Forest Service:

<u>White Legend on Green Background</u>	<u>White Legend on Blue Background</u>	<u>White Legend on Brown Background*</u>
Highway destinations	Motorists' services	All recreational and
Highway mileages	signs (gas, food, etc.)	cultural interest signs
Mileposts	may be used in lieu of	(MUTCD, 2H).
Parking	brown-background	Forest Service
Bike routes	signs (MUTCD, 2E-51).	route markers
Governmental boundaries	Highway facilities such	(MUTCD, 2D-11).
Streams	as rest areas, scenic	General directional guide
Lakes	overlooks and roadside	Signs (MUTCD, 2H).
Mountains	tables (MUTCD, 2E-52).	Historical/geological sites.
Elevations		
Road names		
Airports		

*Within national forests

152.03 Signing Maintained By U.S. Forest Service. Signing to be maintained by U.S. Forest Service, with concurrence of the Transportation Department:

- Forest Service boundaries
- Guard stations
- Administrative sites
- Fire hazard signing
- Timber sales
- Other signing pertaining to administration of national forests
- Point of interest signs

It is suggested that the signs maintained by the Forest Service be covered by an encroachment permit granted to each forest areas.

Requests for other signing not covered above should be coordinated with the State Traffic Engineer.

SECTION 153.00 - SPECIAL SIGNS

153.01 General. Special signs are defined as any sign not included in the ITD Sign Chart. This can include non-standard sign sizes or legends.

Word messages shall conform whenever possible to standard wordings used in the ITD Sign Chart. Symbol designs shall in all cases be essentially like those shown in the MUTCD and the Standard Highway Signs manual.

All special sign requests shall be submitted to the Headquarters Traffic Section for review. Unauthorized signs are not allowed on the right-of-way of any rural or urban highway [Sections [49-805](#) and [40-313\(2\)](#) Idaho Code and Administrative Policy [A-12-01](#)].

Certain signs are permitted on the right-of-way, providing a permit to use the right-of-way has been issued by the Department. A permit is required for both urban and rural areas.

The Transportation Department will furnish, install and maintain authorized signs as indicated herein for other governmental agencies at the agency's expense, unless precluded by a written agreement. However, a letter of payment procedure must be obtained prior to fabrication and erection of the signs. The following guidelines shall apply to signs for other agencies:

- Other agencies will be charged for all ITD fabrication costs.
- If an agency provides the sign, they will be charged for all installation costs, including materials, equipment, and labor.
- Other agencies will be charged for all materials, equipment, and labor needed to replace a sign or post due to normal deterioration, vandalism, or other damage.

Do not install special signs at state highway junctions or at other locations where the signs will interfere with the effectiveness of other official traffic-control signs or devices. Signs duplicating existing signs furnished and installed by the Department will not be permitted.

Fluorescent yellow-green crossing signs for use at pedestrian, bicycle, and school warning crossings are optional to replace the standard yellow warning signs. However, sign colors should not be mixed at a selected site area which will require all crossing signs in the selected area to be installed or changed at the same time.

If an agency requests the optional fluorescent yellow-green warning signs when the current signing is still in satisfactory condition, the requesting agency will be responsible for all signing costs. The Department will participate in the signing costs if the signs are changed from yellow to fluorescent yellow-green when signing is needing replaced due to normal deterioration.

Design all signs to meet MUTCD requirements.

153.02 Signs Eligible for Installation In Rest Areas Or Turnouts That Provide Vehicle Parking.

- Signs regarding notices of community services, civic organizations and churches
- Long-narrative, special-feature and informational signs
- Fish and Game check station signs
- University of Idaho tourism survey signs
- Blue Star Memorial (see Section [153.08.02](#))

Signs can be mounted as either a permanent or as a temporary installation. The signs shall be covered by a permit if the rest area or turnout is part of the highway right-of-way. Temporary installation permits can include a memorandum of understanding, an encroachment permit, or a special event permit (see Sections [153.05](#) and [153.06](#)). The cost of sign installation and maintenance shall be covered in the permit and is normally at the expense of the requester.

153.03 Other Signs Eligible for Installation (when requested and approved) By the Transportation Department.

<u>Maintained at Other Agency's Expense</u>	<u>Maintained by ITD</u>
University of Idaho Research and Extension Centers	Hospital
Department of Commerce Rest Area Information signs (see Section 180.05)	National Guard armories
Wildlife management guide signs	National Engineering & Environmental Lab
Fish and Game guide signs (see Section 181.03)	Refuse disposal operations – landfills
Livestock, health and brand inspections (located at port of entry stations)	Emergency response call numbers
County or District health offices	Experimental program signs
Fire district boundaries	Local ordinance signs (see Section 153.07)
Wildfire reporting signs	Historic trail signs (see Section 187.11)
Wildlife viewing signs (see Section 187.16)	
Gem Community signs (see Section 187.17)	
Storm Ready Community signs	
City Heritage signs	

153.04 Signs Not Authorized for Installation Within Highway Right-Of-Way (refer to the Outdoor Advertising Act as a guide for placing the following signs outside of the highway right-of-way):

- 4-H signs
- Future Farmers of America
- Soil conservation districts
- Water conservation districts
- Irrigation districts
- Bird sanctuary boundaries
- Forest protective districts
- Boy/Girl Scouts
- Anti-drug/alcohol signs
- Special highway/bridge names (see Section [177.05](#) for exceptions)
- Local highway district facilities

An official or public sign or structure erected near a city or county which contains name of city or county, provided it is maintained at public expense, is exempt from the Outdoor Advertising Act provisions.

153.05 Special Signing for Municipal Seasonal Events. In accordance with IDAPA [39.03.42](#), signs, banners and decorations are allowed for short duration on the highway provided that a written request is received from the community and a special events permit has been approved by the District. Signs and banners are NOT allowed on signal mast arms. These events should be classified as major events such as state fairs and major rodeos. Conduct a study to determine the most feasible route and signing to guide motorists to and from the sites.

153.06 Special Signing for Urban Highways. All signs other than official highway signs located in or over the urban highway right-of-way must be covered by a permit issued by either the municipality or the Transportation Department, whichever has the authorization as specified in the Cooperative Agreement for Maintenance of State Highways in the municipality.

153.07 Special Signs Covering City/County/Federal Ordinances. Cities or counties may, with approval, install and maintain ordinance signs at the entrance to the city or county. No sign shall be placed without a supporting local ordinance. Verification of such ordinance shall accompany all sign requests. The signs shall be located and spaced so they will not conflict with ITD-placed signing. The signs shall not be installed with other ITD signs but will be on separate posts with a maximum of two signs per post so they can be read. The signs shall conform to the requirements of the MUTCD for legend design, color, size and mounting. Examples of these signs are:

- Neighborhood Watch
- Speed Checked by Radar
- Green River Ordinance Enforced
- Watch for Pedestrians/Children
- Warning Children in this Community Protected by Child Registering
- Compression Brakes Prohibited
- TERO Ordinance in Effect

153.08 Memorials. Requests for memorials will be processed by the District office. General directional signs to memorials shall meet the requirements of guide signs for interstate or conventional highways.

153.08.01 Traffic Accident Memorials. In accordance with Section [49-1316](#), and [55-2201](#) through [55-2210](#), Idaho Code, and Administrative Rule IDAPA [39.03.63](#), Traffic Accident Memorials may be erected within the right-of-way of highways in memory of people killed in traffic accidents. The following requirements apply:

- No memorial can be placed without a permit approved by the District office.
- Memorials shall not be placed in the interstate or highway median.
- Memorials are not permitted within boundaries of incorporated cities.

153.08.02 Blue Star Memorials. The National Council of State Garden Clubs, Inc. initiated the Blue Star Memorial Highway Marker program. This nationwide movement was started after World War II to pay tribute to the nation's armed forces. The following requirements apply:

- Markers will only be erected on highway segments that have been designated as Blue Star Memorial highways. This designation will be coordinated by the Planning Section.
- Memorial installations shall be covered under an encroachment permit ([JTD-2109](#)).
- The District shall coordinate and approve the installation site. Recommended sites include rest areas, scenic viewpoints, historical sites, or other appropriate areas approved by the District.
- The permittee is responsible for the cost of all labor and materials, including installation, as well as any modification required to facilities to accommodate the marker.
- Markers that are vandalized or accidentally damaged shall be repaired, restored or replaced at the permittee's expense.
- Stars shall be placed as far as practical from the roadway, but must be placed a minimum of 20 feet (6.1 m) from the shoulder where the right-of-way width permits.
- Final location of the star must be approved by the maintenance supervisor assigned to the roadway section.
- Maintenance of the star is the responsibility of the party requesting the star.

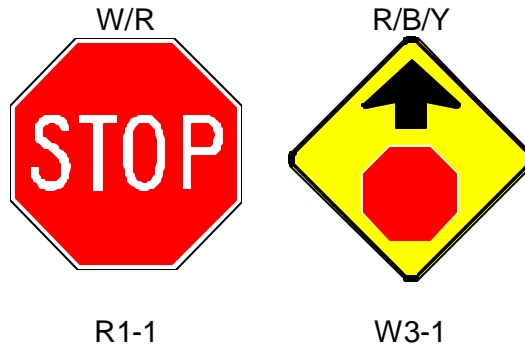
Refer to IDAPA [39.03.63](#) for additional requirements.

153.08.03 Memorial Names for Highways and Bridges. Requests for memorial highway and bridge names require legislative approval. See Idaho Code [40-513](#) and Section [177.05](#).

154.00 Reserved.

SECTION 155.00 - STOP AND YIELD SIGNS

155.01 “STOP” Signs



All segments of the state highways system, including urban extensions, have been designated as through roadways by the Transportation Board [Section [40-310\(11\)](#), Idaho Code]. Install “STOP” signs on all public-road approaches to state highways except where it is determined on the basis of a traffic and engineering investigation that other traffic controls are safer and more desirable.

“STOP” signs may also be installed on high-volume (greater than 500 ADT), private approaches to state highways if needed. Alleys are not considered intersections and “STOP” signs are not required unless other factors dictate their use for safety.

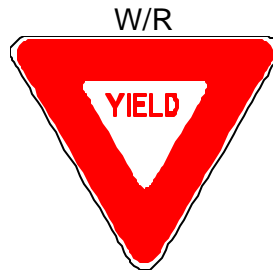
Stop signs at railroad grade crossings are covered in section [851.02](#).

A “STOP AHEAD” sign (W3-1) shall be used on an approach to a stop sign that is not visible for a sufficient distance to permit a driver to bring his vehicle to a stop at the stop sign. A “STOP AHEAD” sign may also be used for emphasis where there is poor compliance with the stop sign.

On local road systems approaching state highways, ITD must share responsibility for installation and maintenance of the “STOP AHEAD” sign with the local jurisdiction. Maintenance agreements with local jurisdictions are encouraged, with ITD furnishing the signs and the local jurisdiction providing the labor. **However, if the local jurisdiction will not or does not install and maintain a warranted “STOP AHEAD” sign on their approach to a state highway, ITD must install and/or maintain one.** All “STOP” and “STOP AHEAD” signs on local roads intersecting the State Highway System shall be inventoried and routinely inspected.

The standard size of stop signs is 30 inches (762 mm) and is the minimum size to be used on approaches to the state highway system. A minimum size of 36 inches (914 mm) should be used for interchange ramp terminals and on high volume approaches that must stop. A 48-inch (1219 mm) stop sign should only be used where stop sign observance needs improvement. The installation of 48-inch (1219 mm) stop signs shall be held to a few selected locations in order to reduce the proliferation of larger signs and restrict the emphasis of larger signs to only needed locations. Additional stop condition emphasis can be provided with dual stop sign installations or stop ahead signing.

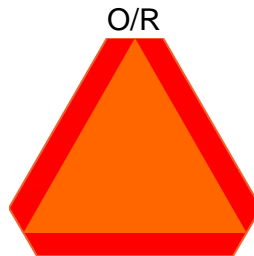
155.02 “YIELD” Signs



R1-2

All “YIELD” signs installed on the state highway system or approaches thereto shall be approved by the State Traffic Engineer prior to installation. Obtain the approval by submitting intersection drawings or sketches which are accompanied by supporting data and recommendations.

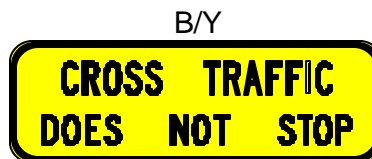
155.03 Slow-Moving Vehicle Emblem



R1-6

The slow-moving vehicle emblem is required by Section [49-619](#), Idaho Code and is to be displayed on all vehicles operating on the state highway system when those vehicles are designed to travel no faster than 25 mph. Snow removal and farm equipment operated on non-interstate roadways are exempt from using the sign. Vehicles displaying the emblem are restricted to highway usage from one-half hour before sunrise to one-half hour after sunset.

155.04 “CROSS TRAFFIC DOES NOT STOP” Signs



W4-4

In compliance with IDAPA 39.03.41, the R1-7 sign has been replaced with the W4-4 “CROSS TRAFFIC DOES NOT STOP” sign. When needed, the W4-4 shall be mounted below the “STOP” sign.

SECTION 156.00 - SPEED LIMIT SIGNS

156.01 Posting Speed Limit Signs



The determination of speed zones is discussed in section [103.00](#).

The following sizes of speed signs are suggested:

- Minimum size: 24" x 30" (610 mm x 762 mm)
- Urban areas and speed limits under 40 mph: 24" x 30" (610 mm x 762 mm)
- Rural conventional highways with posted speed limits 45 mph or greater: 36" x 48" (914 mm x 1219 mm)
- Interstate highways and expressways: 48" x 60" (1219 mm x 1524 mm)

Do not install or relocate "SPEED LIMIT" signs for speed zone changes until the signs are covered by an official speed minute entry.

Posted limits generally should be to the closest 5 mph increment of the 85th percentile speed.

156.01.01 Post speed limits in urban areas as follows:

- Limits of each speed zone.
- Just beyond all junctions of the state highway system.
- Additional signs every three or four blocks, but not to exceed approximately 1,500 feet (450 m).
- City authority to set speed limits within their jurisdiction is covered under [A-12-07](#).

156.01.02 Post speed limits on rural portions of the state highway system as follows:

- Limits of each speed zone.
- On the highway in the vicinity of all primary feeder roads entering the highway system.
- Just beyond all junctions of the state highway system.
- At all state-line entrances.
- Additional signs at approximate 10 mile (16 km) intervals for each direction of travel.

156.01.03 Post speed limits by local jurisdictions:

Placement of speed limit signs on the state highway system by local jurisdictions shall be limited to locations where jurisdictional boundaries occupy both sides of the highway.

156.02 Location. Section 2B-15 of the MUTCD states that speed limit signs “shall be located at the points of change from one speed limit to another.” Therefore, speed changes should be located so that the “SPEED LIMIT” sign can be placed as near as practical $\pm 50'$ (± 15 m) to the official minute entry location. Correlation of the speed signing with other highway signs is very important and there will occasionally be locations where the speed signing will conflict with other signing. In those cases, review the signing carefully in the field and make the appropriate adjustments in the signing and, if necessary, adjustments to the official speed limit minute entry.

Normally, the reduction in speed limit is installed at the point specified in the speed minute entry. The increase in speed limit can be installed downstream within the higher-speed zone.

At rural highway junctions, the “SPEED LIMIT” sign need not be installed right at the junctions even though the minute entry reads from that point. Generally, place the signing beyond the junction in the following order: route marker, speed limit sign and distance sign.

In those cases where a curve occurs in the proximity of the junction, give priority to the curve warning sign over the other signing.

156.03 “REDUCED SPEED AHEAD” Sign. The “REDUCED SPEED AHEAD” is not usually required in urban areas but should be installed for speed reductions approaching a community or on rural speed reductions where the speed reduction is 10 mph or greater. The size of the sign shall correspond with the speed limit sign sizes outlined in Section [156.01](#).

Speed reductions of greater than 20 mph should be accomplished through two speed zone transitions. For example, a single transition can be used from 65 mph to 45 mph, but when going from 65 mph to 35 mph or 25 mph you should first reduce to 45 mph and then to the lower limit. Transition zones should typically be from 0.2 mile (300 m) to 0.5 mile (800 m) in length.

156.04 Maximum Speed Limits at City Limit Boundaries. The following criteria shall be followed when allowing the installation of maximum speed limit signs at the entrance to cities on the State Highway System:



Maximum speed limit signs may be posted at city limit boundaries only when a city has an ordinance for a maximum speed limit in place and all criteria of this section are met. The city shall provide written verification of an existing ordinance prior to sign approval. The ordinance will be acceptable verification that the local jurisdiction has conducted all appropriate engineering studies and traffic investigations required to establish the maximum speed limit. The District shall retain such verification records.

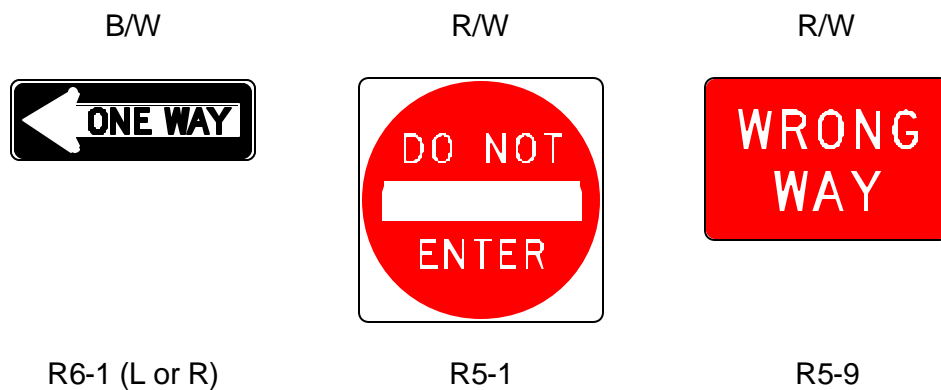
All signs shall be fabricated, installed and maintained by the Department. Signs shall be black on white regulatory signs as shown above. **No signs shall be ordered, fabricated or installed until the Department has a city's ordinance number to place on the signs.**

Maximum speed limit signs shall be installed below speed limit signs that exist at the city limit boundaries. If an existing speed limit sign is not in place, a speed limit sign for the established speed zone at the city limit shall be installed and the maximum speed limit sign shall be mounted below it.

The District will have the option to either adjust the speed zone at the city limit boundaries, using appropriate speed minute entry procedures, or add an additional speed limit sign installation to comply with this policy.

Districts should notify those jurisdictions posting non-conforming maximum speed limit signs for sign removal or, with ordinance verification, sign replacement. Districts should allow cities an appropriate amount of time to establish an ordinance upon their request.

SECTION 157.00 – “ONE WAY”, “DO NOT ENTER” AND “WRONG WAY” SIGNING



157.01 General. Install these signs at all divided or undivided highway ramp terminals and intersections that have one-way traffic control to direct proper one-way movement.

Arrow pavement markings **may** be placed in each lane of a divided highway in advance of a crosswalk and/or at other locations where there are confusing roadway geometrics or to improve driver observance of the one-way movement.

Typical signing for a divided highway having at-grade intersections is shown in Figures [157.01-01](#) and [157.01-02](#).

Typical signing for interchange ramp terminals is shown in Figure [157.01-03](#).

The mounting height of “WRONG WAY” and “DO NOT ENTER” signs on interchange ramps shall be at least 4 feet (1.2 m) above the pavement edge to the bottom of the sign.

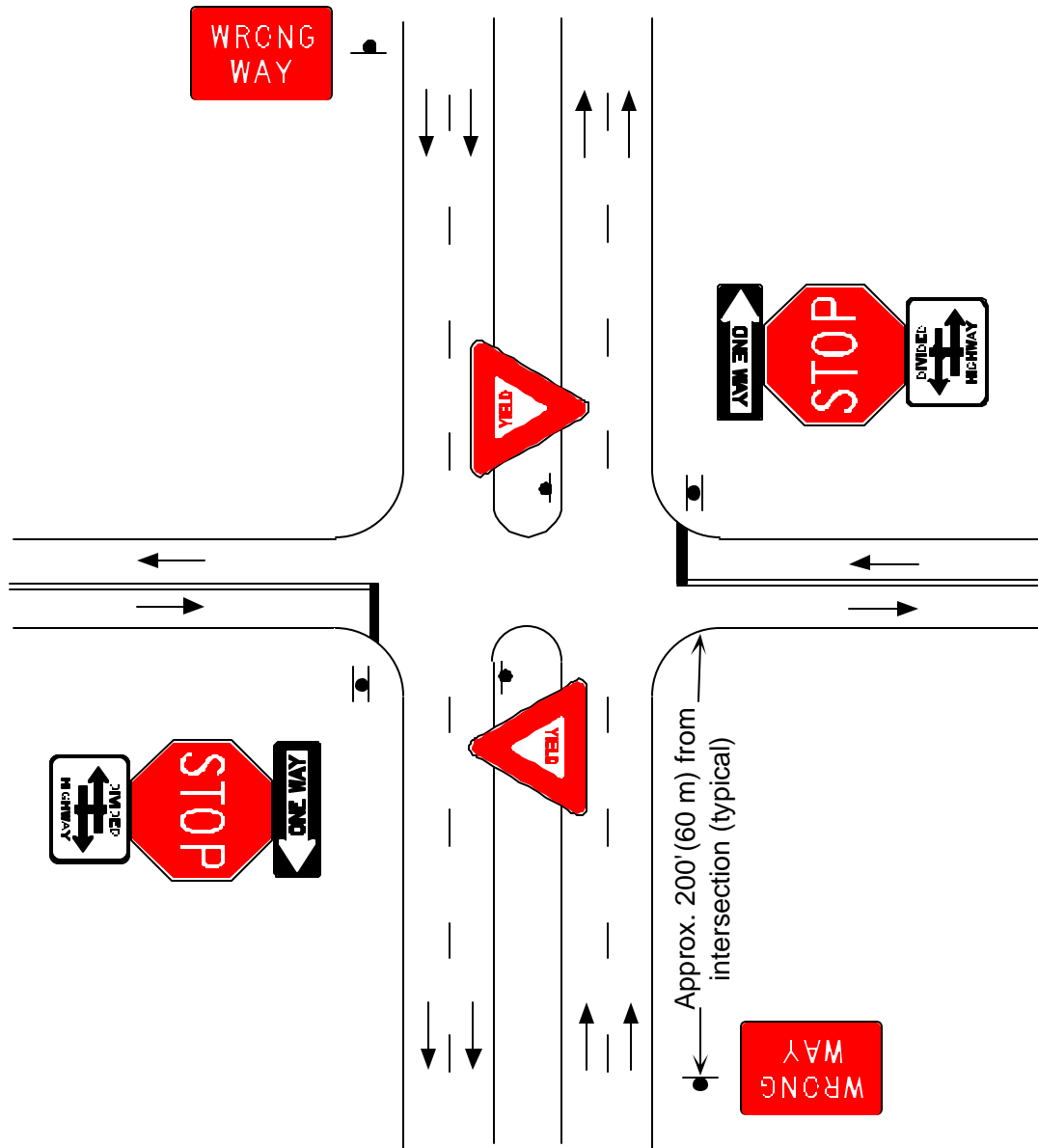


Figure 157.01-01 Typical Signing At Divided Highway Intersections With Median Width Of Less Than 30' (9.1 M)

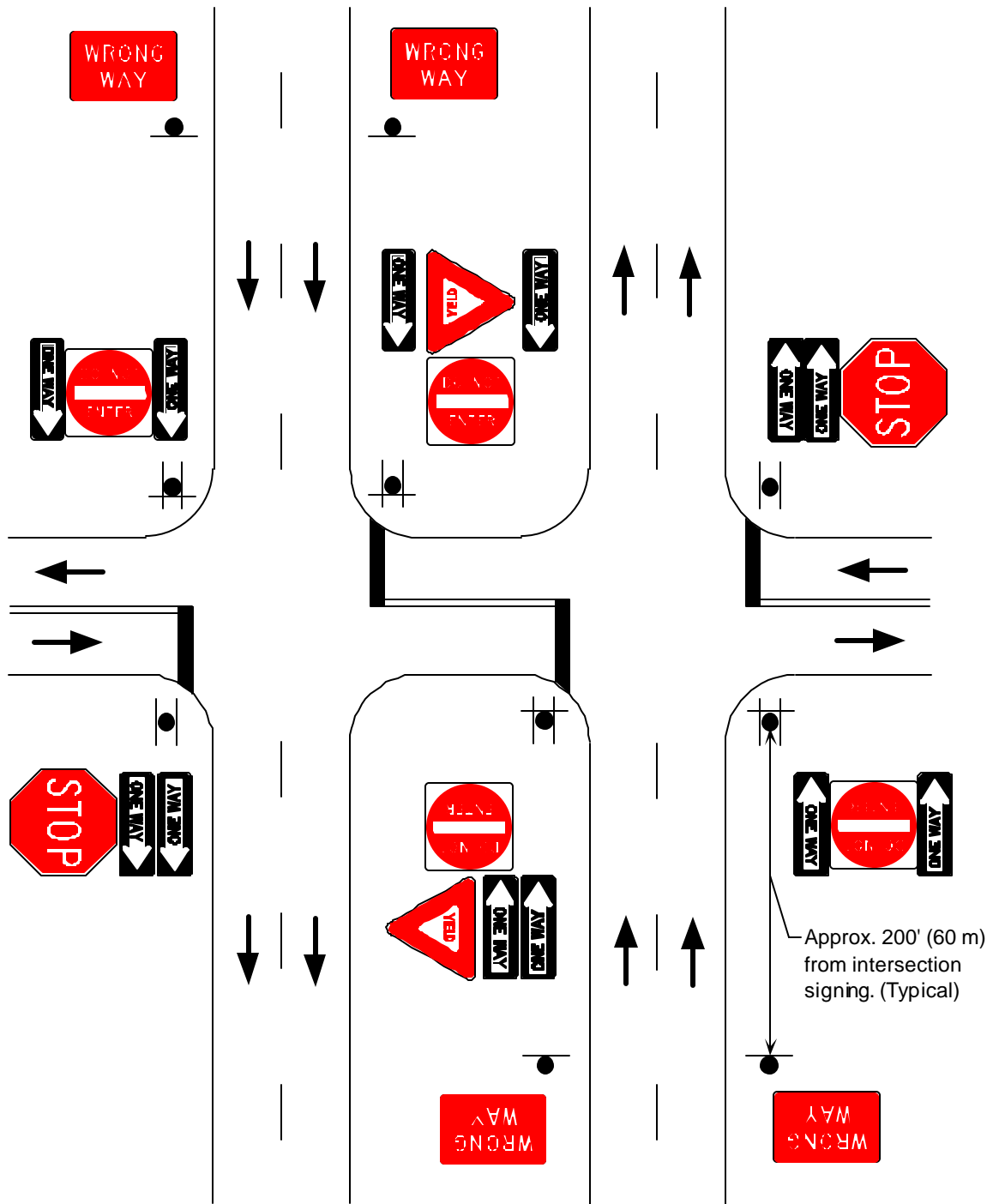


Figure 157.01-02

Typical Signing At Divided Highway Intersections With Median Width of 30' (9.1 M) Or More

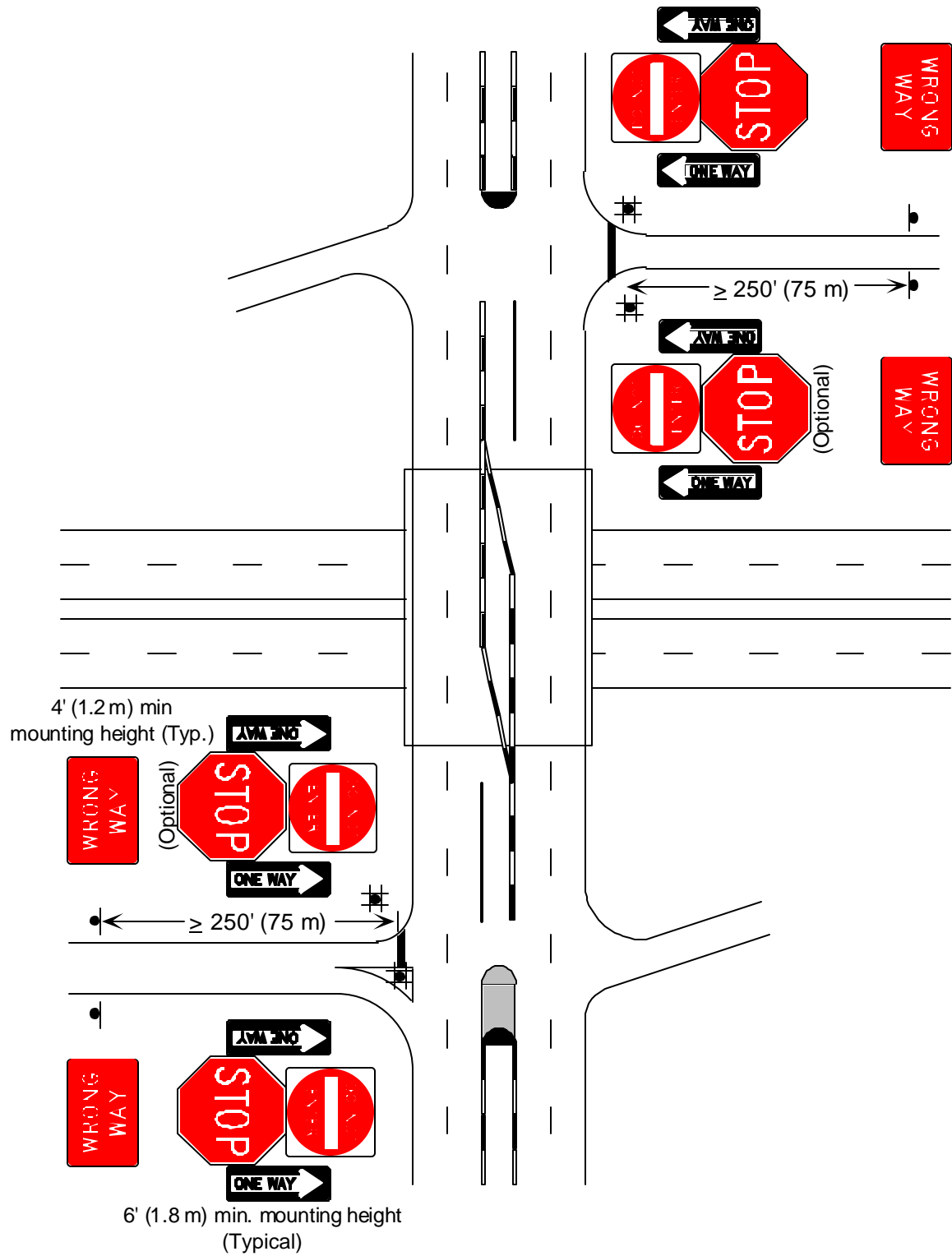
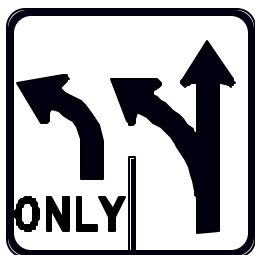


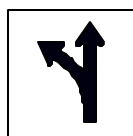
Figure 157.01-03 Typical Signing At Diamond Interchange Divided Crossroads

SECTION 158.00 - LANE USE CONTROL SIGNS

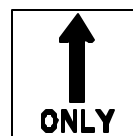
B/W



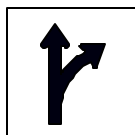
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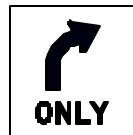
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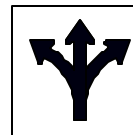
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4

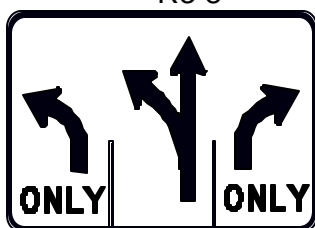


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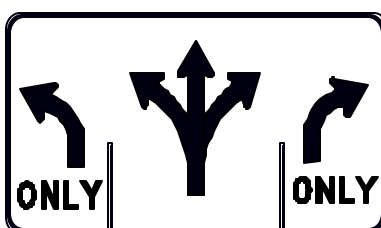


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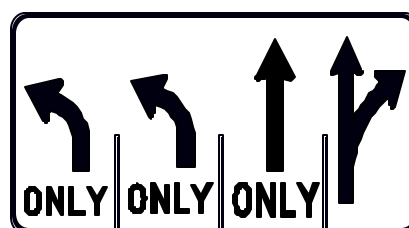
R3-8



R3-8(125)



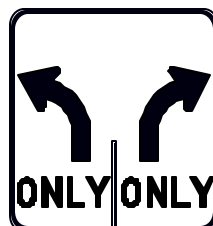
R3-8(165)



R3-8 (1134)



R3-8(11)



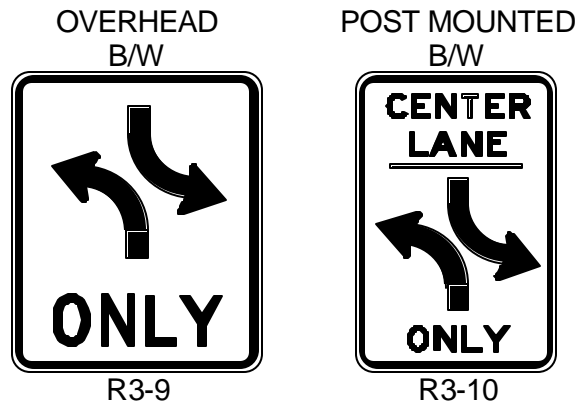
R3-8(15)

EXAMPLES

158.01 General. Use the standard R3-8 sign to direct lane use at intersections, when applicable.

At intersections with modified lane use and additional lanes, modify the R3-8 sign number to reflect the actual lane use combination.

SECTION 160.00 - TWO-WAY LEFT-TURN SIGNS



160.01 General. Use two-way left-turn-only signs (R3-9 or R3-10) where a lane in the center of a highway is reserved for the exclusive use of left-turning vehicles in either direction and is not used for passing and overtaking. Use pavement markings in conjunction with these signs. The R3-9 sign is mounted overhead and centered over the two-way left-turn lane whenever possible. Use the post-mounted R3-10 sign as an alternate or supplement to the R3-9 sign.

Typical signing and pavement markings for two-way left-turn lanes are shown in Figures [202.05-01](#) and [202.05-02](#).

SECTION 161.00 - MISCELLANEOUS REGULATORY SIGNS

161.01 “THRU TRAFFIC KEEP RIGHT” Or “THRU TRAFFIC KEEP LEFT”



The “THRU TRAFFIC KEEP RIGHT” sign may be installed in advance of intersections to inform motorists that the right lane is for traffic that desires to continue through the intersection.

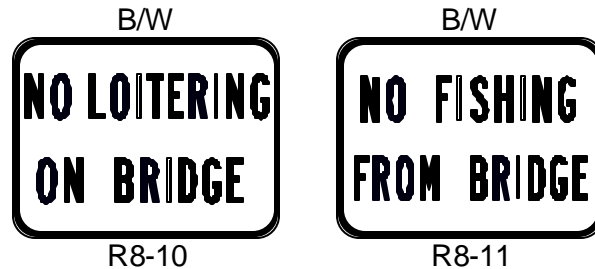
This sign may also be used to supplement overhead lane-use control signs when the left-turn lane is not protected with a median and an exclusive right-turn lane is not present. Use the “THRU TRAFFIC KEEP LEFT” sign in the same manner where reverse conditions exist.

161.02 “KEEP RIGHT OF ISLAND”



The “KEEP RIGHT OF ISLAND” sign should be used at the beginning of painted roadway medians where it is not desirable to install a symbol sign in the median. Mount this sign not more than 50 feet (15 m) before the approach end of the median on the right side of the roadway.

161.03 “NO LOITERING ON BRIDGE” Or “NO FISHING FROM BRIDGE”



Install the “NO LOITERING ON BRIDGE” or “NO FISHING FROM BRIDGE” signs on bridges where an investigation shows a hazardous condition exists because of pedestrian activities on the bridge.

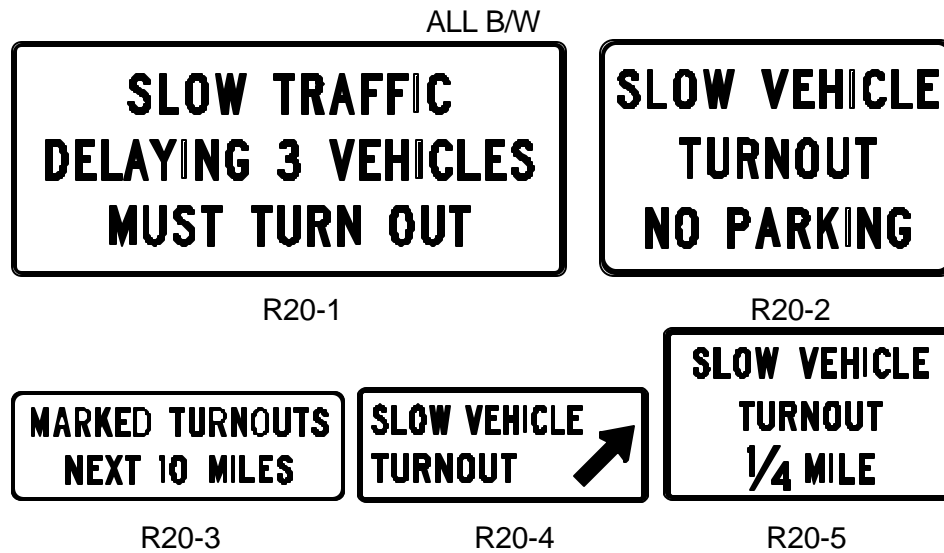
161.04 Pedestrian Signal Signs



See Section [308.06](#) and Standard Drawing I-6-A.

The “Pedestrian Message Decal” is an adhesive-backed decal that may be placed on signal poles to provide additional information for pedestrians. It may be particularly useful where children and the elderly cross frequently or where there have been complaints about the operation. It is generally applied above and centered between the pedestrian push buttons. These are available from Headquarters Traffic.

161.05 "SLOW VEHICLE TURNOUT" Signs



Install the "SLOW VEHICLE TURNOUT" signs on two-lane highways that have suitable turnouts, where traffic volumes are high and/or passing opportunities are limited. The signs are especially useful on high volume recreational routes. The signs are authorized under Section [49-639](#), Idaho Code. Typical signing and pavement markings for slow vehicle turnouts are shown in Figure [161.06-01](#).

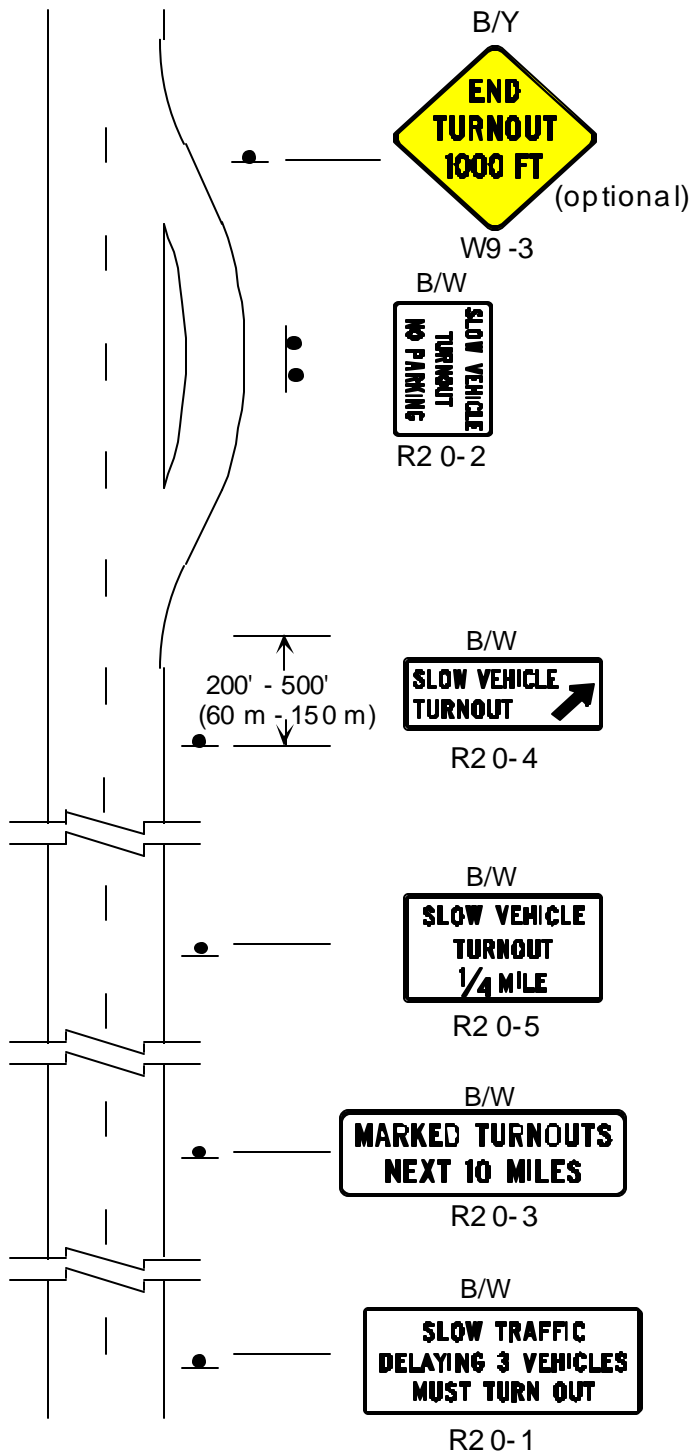
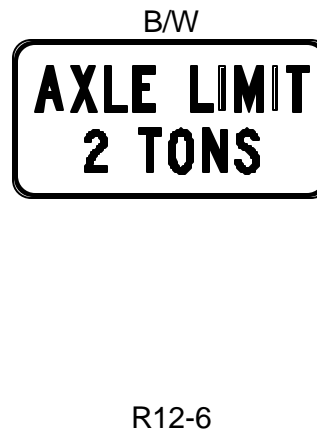


Figure 161.06-01 Slow Moving Vehicle Turnouts

161.06 Truck Weight Limit Signs



Install the truck weight limit signs on those structures that have been selected as critical bridges based on the Bridge Inspection Program. Coordinate and obtain approval from the Bridge Section for the appropriate weight limits on specific bridges. Use these signs with the warning sign W8-15; refer to Section [167.04](#).

161.07 Handicapped Parking Signs



When parking spaces for the handicapped are marked in rest areas (see Section [202.16](#)), use the “RESERVED PARKING” sign (R7-11) (Section [49-213](#), Idaho Code). There should be a single-headed arrow pointing in the direction the regulation is in effect, if the sign is at the end of a zone, or a double-headed arrow pointing both ways, if the sign is at an intermediate point in a zone.

Where a guide sign is needed to direct motorists to van-accessible parking facilities, the “VAN ACCESSIBLE” sign (R7-14) will be installed under the “RESERVED PARKING” sign (R7-11).

161.08 Parking Prohibition Signs



Idaho Code [49-202\(28\)](#) gives ITD the authority for “prohibiting, limiting or restricting the stopping, standing or parking of vehicles on any (state) highway where such stopping, standing or parking is dangerous to those using the highway or where the stopping, standing or parking of vehicles unduly interferes with the free movement of traffic thereon.” Guidelines for parking minute entries are discussed in Section [104.00](#).

Rural parking restrictions on the state highway system require traffic minute entries in accordance with Administrative Rule [39.03.65](#) and Administrative Policy [A-12-03](#). Regulation of parking on the state highway system within incorporated communities requires the concurrence of the municipal officials and shall be covered by a local ordinance rather than a minute entry.

Refer to Sections 2B-34 through 2B-36 of the MUTCD for the appropriate use and placement of the various parking prohibition signs.

161.09 “COMPRESSION BRAKES PROHIBITED”

B/W



R5-201

Install the “COMPRESSION BRAKES PROHIBITED” signs only when a city or county has a local ordinance in effect. Verification of an existing ordinance must be submitted with the sign request to the District. The ordinance number shall be placed on the sign. The District shall approve all installation locations.

162.00 Reserved.

SECTION 163.00 - PASSING ZONE SIGNING

163.01 General. No-passing zones shall be established on all state highways at limited sight distance locations on vertical and horizontal curves.

Idaho Code [49-202\(23\)](#) authorizes the Idaho Transportation Department or local authorities to determine and install appropriate signs and markings for no-passing zones.

No-passing zones are officially marked by the barrier pavement stripe. The Districts shall measure, mark and inventory these zones on all state highways. These zones should be checked periodically for proper location and changes made where necessary. All new sections of road should be inventoried and no-passing zones established where needed. See Section [201.03](#), No Passing Zones.

The pavement stripe is the official marking of no-passing zones. Where additional emphasis is needed or where the markings are ineffective, use the “NO PASSING ZONE” pennant sign (W14-3). If even greater emphasis is needed, then use the “DO NOT PASS” (R4-1) and the companion “PASS WITH CARE” (R4-2).

163.02 “NO PASSING ZONE” Sign

B/Y



W14-3

Install the “NO PASSING ZONE” sign where additional emphasis is needed for the pavement markings or where the markings are ineffective such as over the crest of a vertical curve. Place it on the left side of the highway at or before the beginning of the No Passing Zone.

Do not over use! In mountainous areas only limited use is expected - on level sections it can be used more frequently.

163.03 “DO NOT PASS” And “PASS WITH CARE” Signs

B/W



R4-1

B/W



R4-2

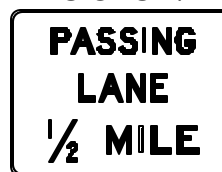
These are additional emphasis signs for no-passing zones where the normal no-passing markings and the pennant no-passing zone sign are not considered adequate.

The “PASS WITH CARE” sign should be used at the end of a no-passing zone where a “DO NOT PASS” sign has been erected at the beginning of the zone.

The “DO NOT PASS” sign is frequently used at the beginning of the roadway segment that has a series of no-passing zones with the “PASS WITH CARE” sign installed at the end of this zone.

163.04 “Passing Lane” And Uphill Climbing Lane Signs

ALL SIGNS B/W



ALL R4-11



R4-12



R4-15

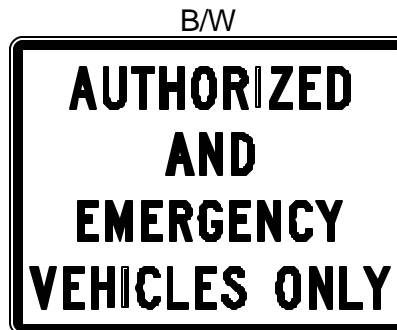
Install advance “PASSING LANE” (R4-11 and R4-12) signs a minimum of 500 feet (150 m) preceding the taper transition. Install “YIELD CENTER LANE TO OPPOSING TRAFFIC” (R4-15) signs when passing in opposing traffic lane is allowed. Refer to Figures [202.01-01.1](#), [202.11-02](#) and [202.11-03](#) for signing and pavement marking details.

SECTION 164.00 - MEDIAN CROSSOVER SIGNING

164.01 Authority. Median crossovers are regarded as additional access points on the interstate highway system. Federal Highway Administration (FHWA) approval of ALL modifications to access control on interstate highways shall be obtained prior to beginning any event that requires access modification or a break of the access control fence. All requests for access modification on interstate highways will be coordinated through the State Traffic Engineer in writing to the FHWA Idaho Division Administration. For guidance, see 23 CFR 630 and website <http://www.fhwa.dot.gov/legisregs/directives/fapg/0630csup.htm>.

Only those vehicles authorized under the authority of Administrative Policy [A-12-19](#) shall be allowed to use median crossovers.

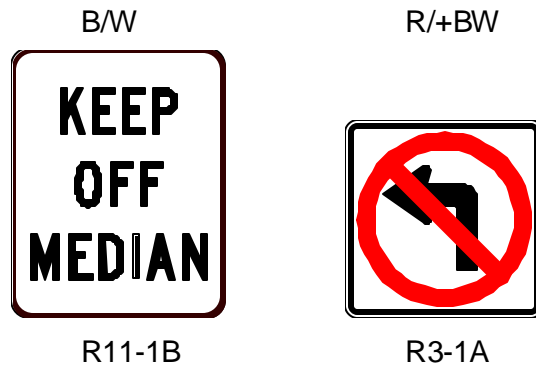
164.02 “AUTHORIZED AND EMERGENCY VEHICLES ONLY” Sign



R8-8

Install “AUTHORIZED AND EMERGENCY VEHICLES ONLY” signs at all FHWA approved freeway median crossovers. Place the sign in accordance with Standard Drawing A-7. The face of the sign shall be perpendicular to the highway centerline. In situations where the median is extremely wide, an additional sign may be required to provide a sign at each entrance to the median. Mount the bottom of the sign at 7 feet (2.1 m) minimum above the adjacent top surface of the crossover road.

164.03 Optional Median Crossover Signing



At median locations that have a history of improper vehicle use, place “KEEP OFF MEDIAN” (R11-1B) and “NO LEFT TURN” (R3-1AL) signs as indicated in Figure 164.03-01.

Sign configurations illustrated in Figure [164.03-01](#) may require modification for other crossover designs (see Standard Drawing A-7).

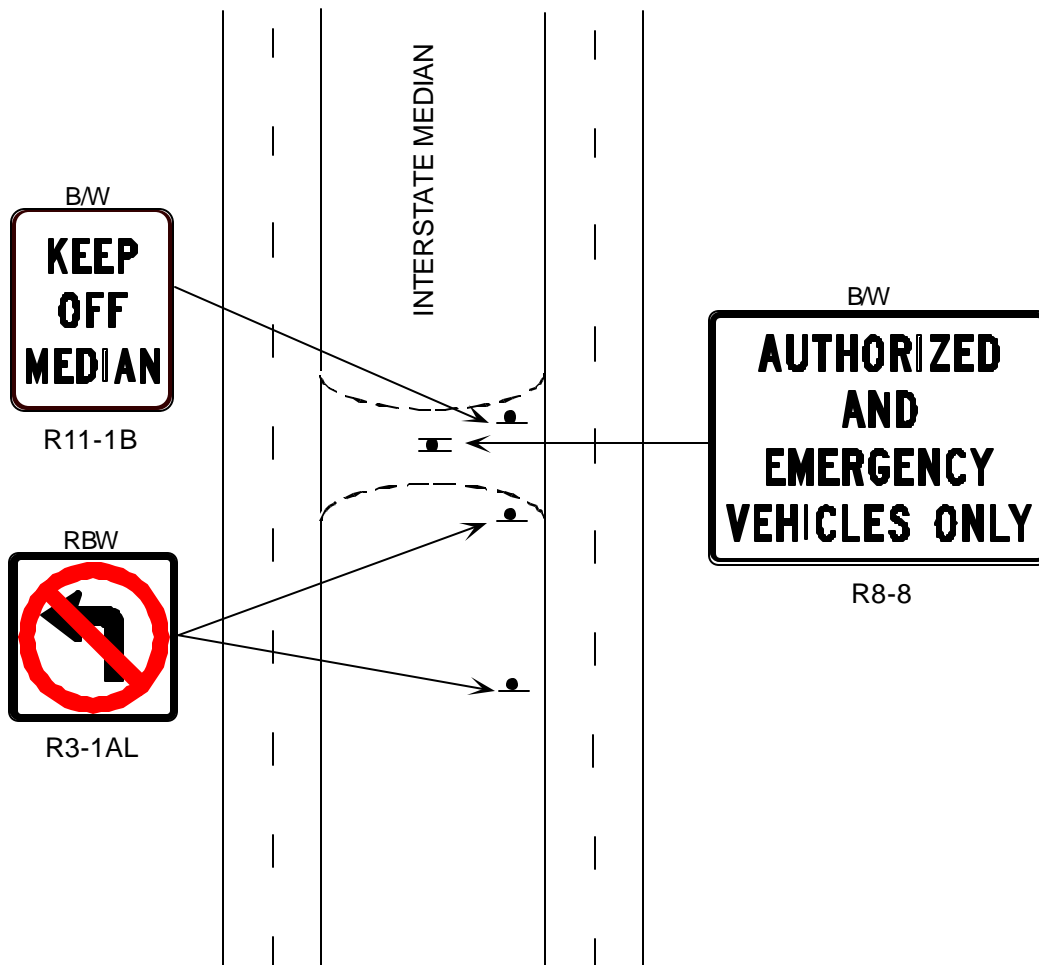
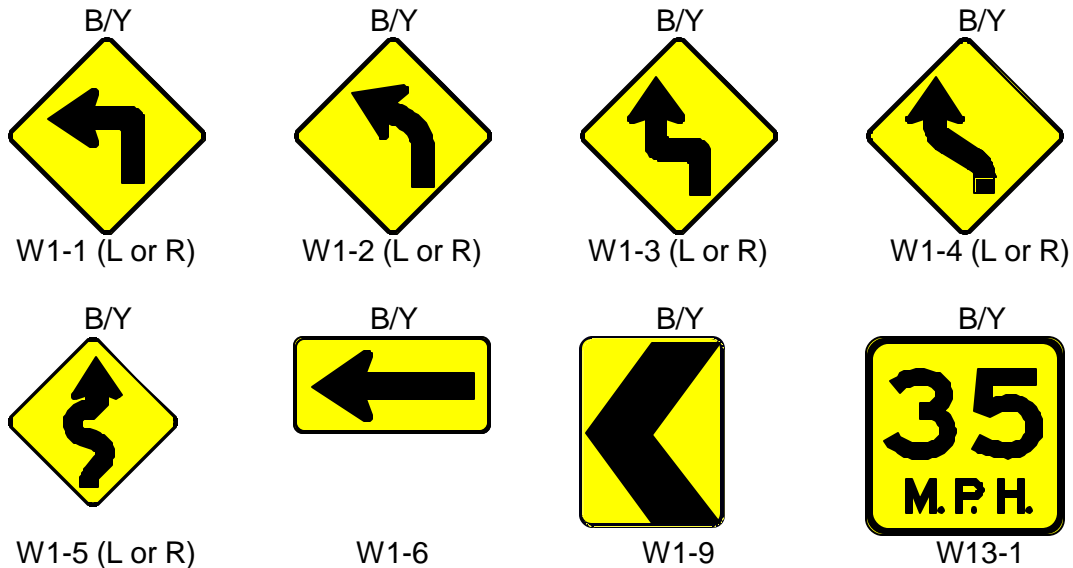


Figure 164.03-01 Median Crossover Signing

SECTION 165.00 - CURVE SIGNING



165.01 General. Curve signs are one of the most demanding and important warning signs. Drivers, especially on state highways, expect to be given nearly complete information on their driving task by signs. This requires that an inventory be made of the curve signing, with all curves studied and appropriate signs placed to warn of conditions that require driver action. Curve signs as used in this manual are considered to be any of the following warning signs: W1-1, W1-2, W1-3, W1-4, W1-5, W1-6, W1-9 and W13-1.

The following procedures and guidelines should be used for designing curve signing on the state highway system and may exceed the minimum requirements shown in the MUTCD. It is not inferred that these requirements are applicable to local roadways and streets.

165.02 Rules for Signing Curves. The general practice for signing curves will be to sign only those curves that have a safe speed equal to or less than the posted speed limit or to advise motorists of curvature ahead that requires a lower speed than that which might reasonably be expected. Observe the following rules as closely as possible to obtain uniformity in curve signing throughout the state.

A curve sign or turn sign will normally be used without an advisory speed sign when the safe speed of the curve is the same as the posted speed limit.

Advisory speed signs should be used when the safe speed of the curve is 5 mph or more below the posted speed limit.

The minimum size for curve signs and advisory speed signs is given in Section [165.04](#).

Larger signs may be used when there is a high accident history, or where there is a surprise element to the motorist, as determined from field observations and application of engineering judgment. Also, curve warning signs may be supplemented with the large arrow sign (W1-6) or chevron alignment signs (W1-8) when added emphasis is needed.

Avoid changes in speed limits near or adjacent to curves requiring warning signs whenever possible. It is desirable to include the curve and curve warning signs all in one speed limit zone. Since curve warning signs can only be moved a limited distance from their ideal location, the speed zones should be adjusted to avoid this conflict when feasible. If the speed zone location is changed by more than 50 feet (15.2 m), a new speed minute entry must be submitted.

Curve warning signs should be reviewed in the field during both daylight and darkness after installation.

The following rules shall apply when it has been determined that curve warning signs are needed:

- Single Curve or Turn

Sign all single curves or turns for both directions. If the safe speed in both directions is not equal, sign both directions for the lower safe speed.

- Two Curves or Turns

- o Sign two curves or turns separated by a tangent distance of 0.25 mile (400 m) or more as separate curves or turns.
- o Sign two curves separated by 0.12 - 0.25 mile (200 m - 400 m) as separate curves or turns, but the advisory speeds will depend on the slowest curve. The safe speed of both curves is determined normally; signing then will be based on the lower safe speed, with only 5 mph difference between the two curves' posted advisory speeds. For example, if the safe speed on the first curve was 45 mph and the safe speed on the second curve was 25 mph, sign 30 mph on the first curve and 25 mph on the second.
- o Use a reverse curve or turn sign where two curves or turns are separated by a tangent distance less than 0.12 mile (200 m). Base the safe speed on the slowest curve or turn.

- Winding Road Section

- o Use the safe speed of the slowest curve within the winding road section as the advisory safe speed for the winding road section.
- o The following items should be used to modify and supplement the normal winding road section signing.

- Two winding road sections having the same advisory speeds and separated by a tangent distance less than 0.12 mile (200 m) may be signed as one continuous winding road section.

- If a curve within a winding section is extra sharp, sign it separately. Raise the advisory speed for the winding road section to correspond with the safe speed of the next slowest curve. Establish the sign size for the individual curves within the section according to the safe speed of the curve with the safe speed of the winding road section used as the posted speed in the sign size chart, Section [165.04](#).

- A curve or turn sign may be erected following the "Winding Road" sign to indicate the direction of the first curve if its direction is not obvious. Determine the curve or turn sign size according to the safe speed of the first curve. The direction of the first curve in a winding road section may also be indicated by use of a large arrow sign (W1-6) or chevron alignment sign (W1-9) if this additional emphasis is required.

165.03 Determination of Advisory Safe Speed. Record the advisory safe speed for the curves on any particular section of highway on form ITD-1964 (Figure [165.03-01](#)). This form should be completed on all field curve safe speed determinations and retained in the district files as documentation of compliance with traffic engineering practices. This speed is determined as follows:

First log and record the milepost of the PC and PT of all curves, bridges, intersections, railroad crossings, etc.

Determine the advisory safe speeds for the curves by ball-bank indicator readings. Record these on form [ITD-1964](#). The advisory safe speed for any particular curve is that speed at which the following ball-bank reading is indicated:

- 10 degrees (for speeds 35 mph and higher)
- 12½ degrees (for speeds of 25 mph and 30 mph)
- 15 degrees (for speeds 20 mph and below)

Some suggestions for field determination of advisory safe curve speeds are:

- Keep car in balance by evenly distributing load and occupants, checking tire pressure, etc.
- Calibrate the accuracy of the vehicle speedometer reading by making three time-and-distance determinations over a range of speeds.
- Run the curves as smoothly as possible. Maintain a distance of approximately 2 feet (600 mm) between the roadway centerline or lane line if multi-lane highway and the left vehicle wheels throughout the length of the curve.
- Determine the advisory safe-speed readings for each direction of travel on all curves.
- Read the ball-bank indicator and speedometer directly from the front since side-view readings are incorrect.
- Record ball-bank readings in 2½-degree increments. (A 2½-degree increment is approximately equal to a 5 mph speed differential.)
- When determining advisory safe speeds for curves on multilane roadways, use the safe speed for the most critical lane.

Curve Advisory Safe Speed Determination Field Data Sheet

[illegible]

Figure 165.03-01

Curve Advisory Safe Speed Determination Field Data Sheet (ITD-1964)

165.04 Curve Sign Sizes. Three sizes of curve signs are used to show horizontal curvature, and two sizes are used to indicate the advisory safe operating speed. The 18" x 18" (457 mm x 457 mm) W13-1 advisory speed sign shall be phased out and replaced with the 24" x 24" (610 mm by 610 mm) W13-1A advisory speed sign for use with 30" (762 mm) curve signs and 30" (762 mm) turn signs. The 24" x 24" (610 mm by 610 mm) advisory speed sign may also be used with the 36" (914 mm) and 48" (1219 mm) curve and turn signs.

The minimum size of curve and turn signs is 36" (914 mm) on state highways. The proper sizes to use on any particular curve will depend on the posted speed limit of the highway and the determined advisory safe speeds for the curve.

Figure [165.04-01](#) gives the recommended minimum sizes of signs for combinations of posted speed and advisory safe speed of curves on the state highway system. Larger signs may be used when a traffic engineering study indicates the need.































































































































CURVE SIGNS									TURN SIGNS						
Advisory safe speed of curve									Advisory safe speed of turn						
POSTED SPEED LIMIT	MPH	70	65	60	55	50	45	40	35	POSTED SPEED LIMIT	MPH	30	25	20	15
	70		 	 	 	 	 	 	 		55	 	 	 	 
	65		 	 	 	 	 	 	 		50	 	 	 	 
	60			 	 	 	 	 	 		45	 	 	 	 
	55				 	 	 	 	 		40	 	 	 	 
	50					 	 	 	 		35	 	 	 	 
	45						 	 	 		30	 	 	 	 
	40							 	 		25		 	 	 
	35								 		20			 	

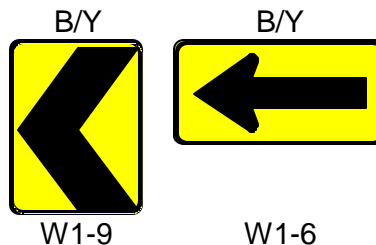
Figure 165.04-01 Recommended Minimum Size Signs For Curves And Turns Inches (MM)

165.05 180-Degree Arrow Curve Sign



The 180-degree arrow (W1-8) is warranted where a curve produces a central angle of 135 degrees or more and a safe speed of 30 mph or less. If the curve can be driven at a safe speed over 30 mph, use a standard curve warning sign instead of this sign.

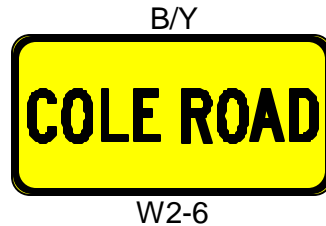
165.06 Chevron Alignment Signs and Large Arrow Signs



Chevron alignment signs (W1-9) and large arrow signs (W1-6) may be used to supplement standard curve signing and delineation where field observation or accident records indicate that additional curve warning is needed to make the curve more apparent to approaching motorists. Normally, use chevron alignment signs when additional emphasis is needed with curve signing at curves with 35 MPH or higher advisory safe speeds, and use the large arrow signs to supplement turn signs when needed for curves with 30 MPH or less advisory safe speeds. When chevron alignment signs are used, they should be spaced close enough together and enough used to make the alignment of the curve apparent to motorists both in daylight and darkness. At least three chevron signs should be visible in a car's headlights at all points on the curve. A spacing of one-fifth the radius of the curve is a suggested guideline.

If the chevron alignment signs cannot be spaced properly because of approaches on the outside of the curve or for other reasons, the "large arrow" sign (W1-6) may be used instead of the chevron alignment signs.

SECTION 166.00 - INTERSECTION SIGNS



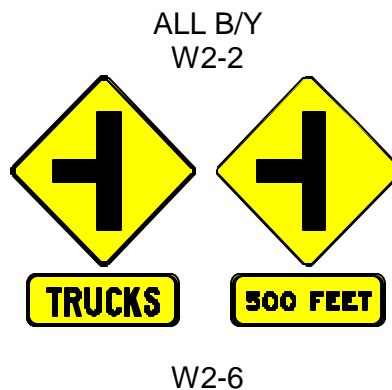
166.01 Intersection Road Name Signs. The intersection road name sign may be used as a supplemental sign below an intersection symbol sign (W2-1 through W2-4). Legend will be 6 inch (150 mm) text. Supplemental plaques will incorporate abbreviated text as necessary to prevent sign width from exceeding primary sign width.

This sign is intended for use on more important local roads. The sign should not be used on minor or unpaved roads unless they are of special local significance.

Use only where the local jurisdiction has officially named the road with some assurance that the road name is correct to avoid local disagreements on road names.

Refer to MUTCD 2C-39 through 2C-45 for use of intersecting road signs.

166.02 Entrance Signs



Where a need is established, entrances to theaters, factories, businesses, cemeteries and other traffic generators may be marked with an appropriate intersection sign, such as the “T” symbol (W2-2) sign. A supplemental plaque (W2-6) indicating the distance to the source of congestion may be placed below the intersection sign. Limit entrance signs to locations where the concentration of high-volume traffic, restricted sight distance and high speeds create a potentially hazardous condition not readily apparent to the highway user. Because of the large number of potential locations for the signs, exercise judgment in order to hold the number of signs to a minimum.

SECTION 167.00 - SPECIAL BRIDGE SIGNING

167.01 “ONE-LANE BRIDGE”

ALL B/Y

W5-3



W13-1

The “ONE-LANE BRIDGE” sign should be used in advance of bridges having less than 18 feet (5.5 m) of clear roadway width.

Figure [167.01-01](#) illustrates the signing, delineation, and pavement markings that are recommended for bridges under 18 feet (5.5 m) in width on the state highway system.

In speed zones 35 MPH and higher, the W13-1 Advisory Speed Plate for 35 MPH may be used with the “ONE-LANE BRIDGE” sign.

167.02 "ONE-LANE BRIDGE FOR TRUCKS AND BUSES"

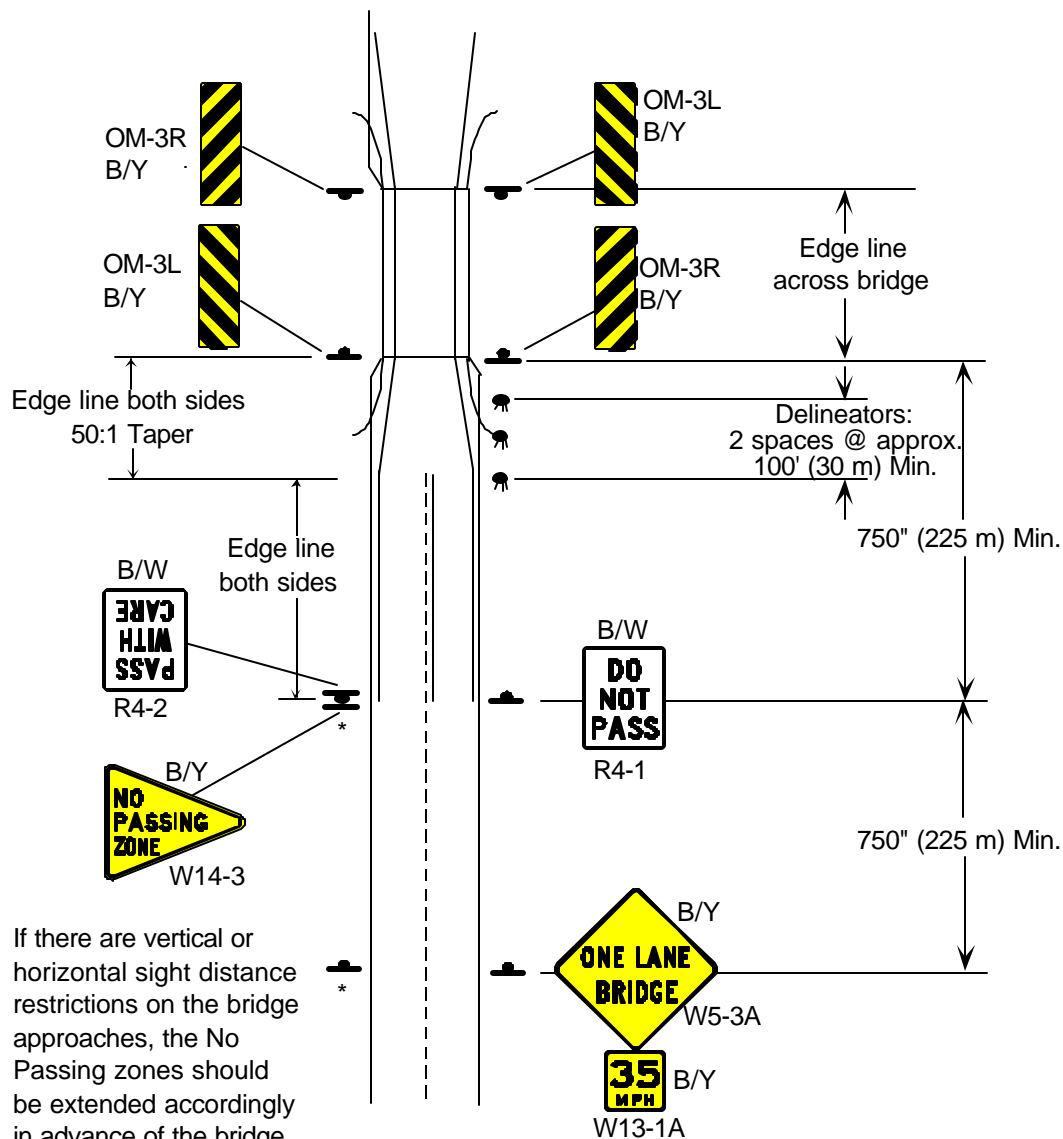
ALL B/Y

W5-3



W13-1

Use the "ONE-LANE BRIDGE FOR TRUCKS AND BUSES" sign on those structures having a clear two-way roadway width of 18 feet (5.5 m) or over but less than 22 feet (6.7 m).



* Optional

Figure 167.01-01 Signing And Pavement Markings For Bridges 18' (5.5 M) Or Less In Width

Bridges with poor immediate approach alignment of insufficient width to permit the safe two-way movement of trucks and/or buses but sufficient width to permit the safe two-way movement of passenger cars, shall be marked with this sign. Figure [167.02-01](#) illustrates the standard procedure for signing, delineation and pavement markings for these widths of narrow bridges.

In speed zones of 35 MPH and higher, the W13-1 Advisory Speed Plate for 35 MPH may be used with the "ONE-LANE BRIDGE FOR TRUCKS AND BUSES" sign.

167.03 “NARROW BRIDGE”



The “NARROW BRIDGE” (W5-2) sign should be used in advance of a bridge or culvert having a clear roadway width of 22 feet (6.7 m) but less than 28 feet (8.5 m) with a clear bridge width less than the approach roadway width.

Figure [167.03-01](#) illustrates the signing, delineation and pavement markings suggested for bridges with clear widths of 22 feet (6.7 m) and over.

The recommended minimum size for “NARROW BRIDGE” signs is 36" (914 mm) for bridges less than 24 feet (7.3 m) wide and 30" (762 mm) for all others.

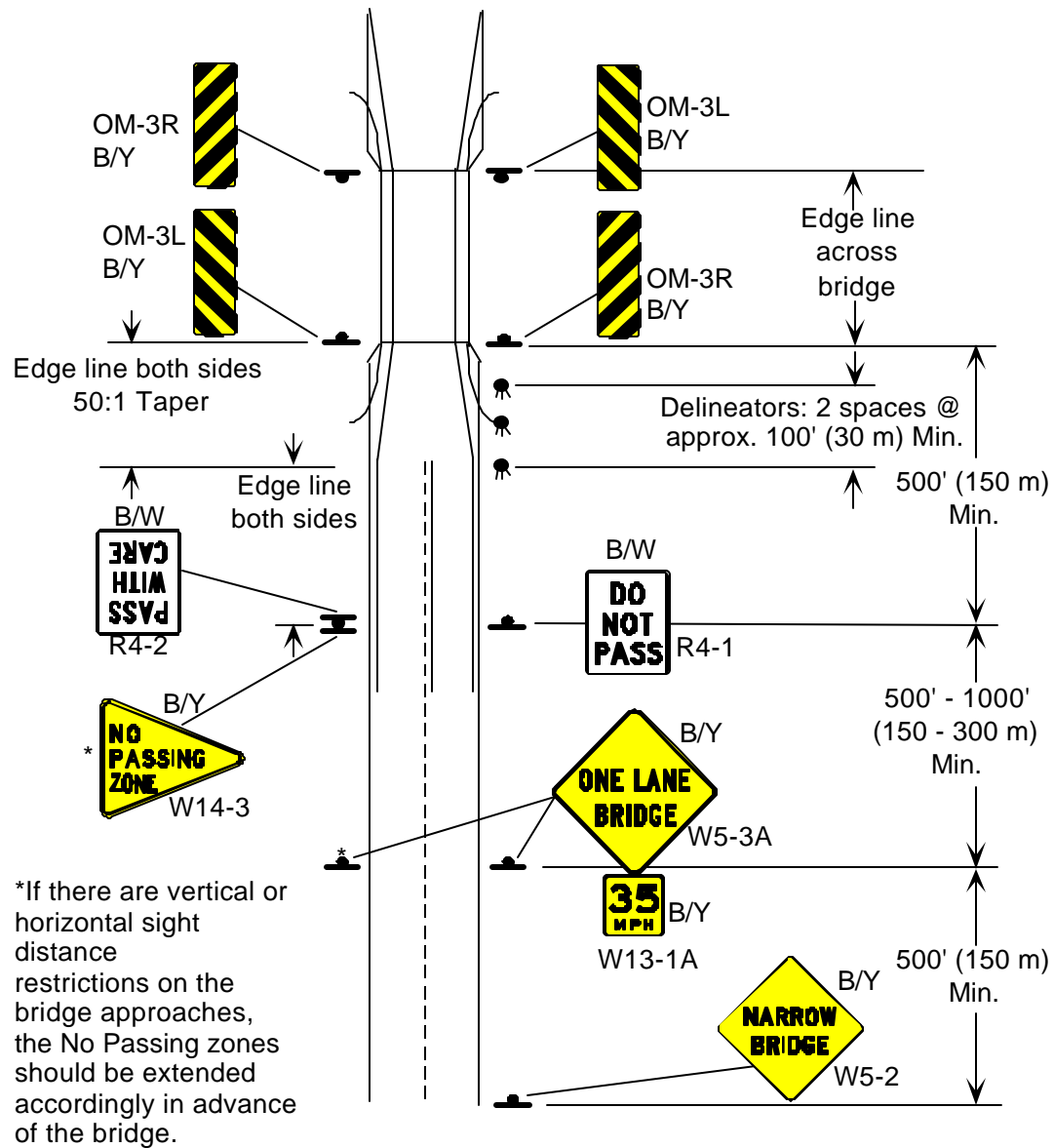


Figure 167.02-01 **Signing And Pavement Markings For Bridges 18' – 22' (5.5 M – 6.7 M) In Width**

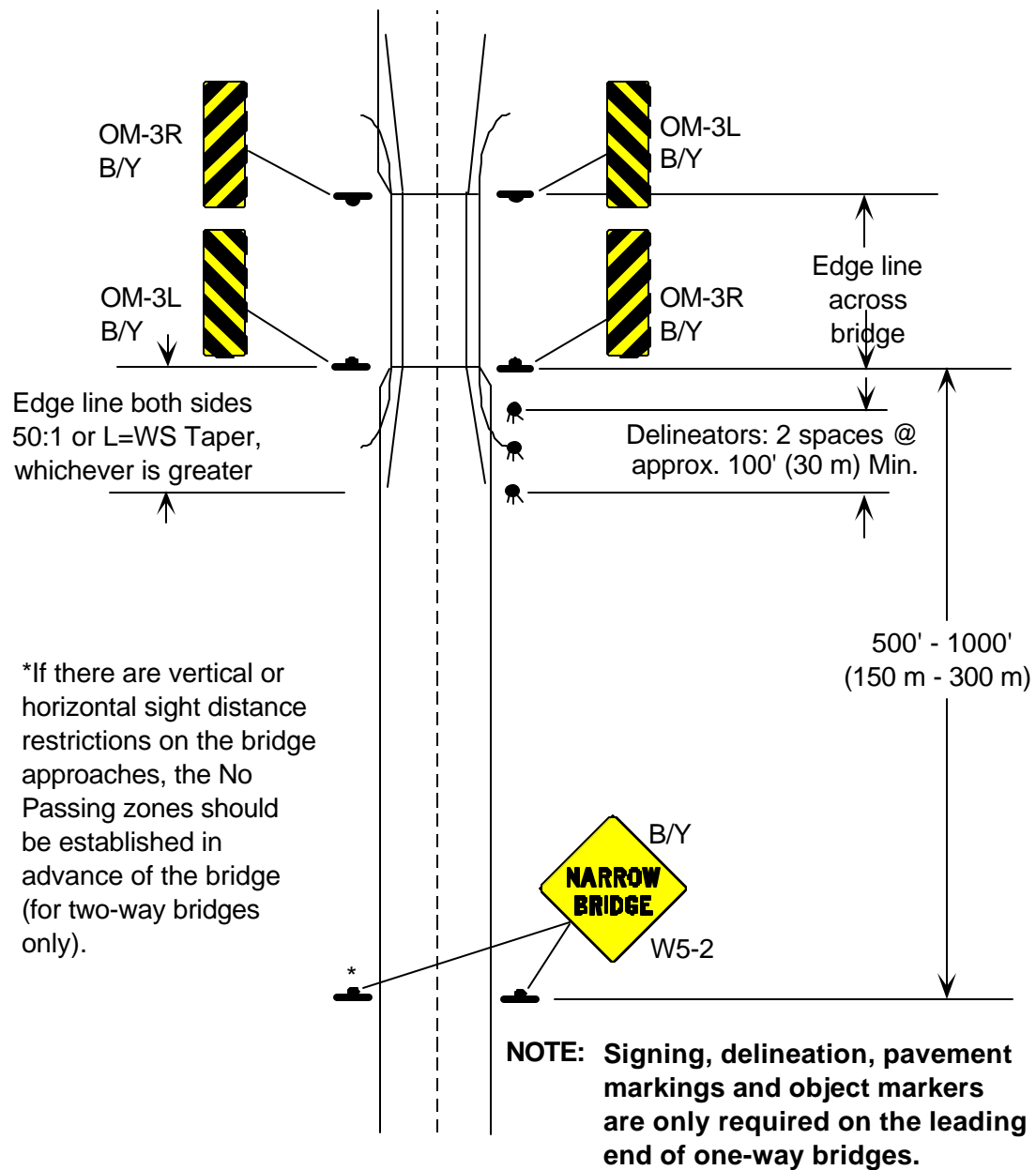


Figure 167.03-01

Signing And Pavement Markings For Bridges 22' (6.7 M) And Over In Width

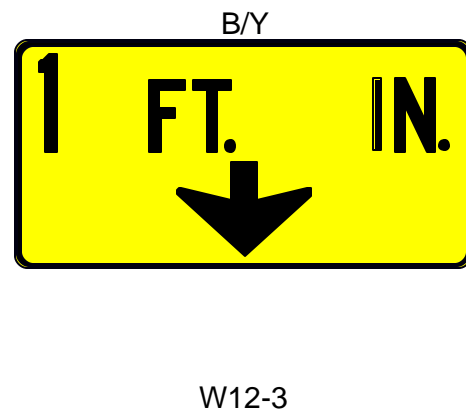
167.04 "RESTRICTED BRIDGE"



Install the "RESTRICTED BRIDGE" sign in advance of structures with weight restrictions, as determined by the Bridge Section. This sign shall always be used in conjunction with the regulatory signs R12-1 through R12-6. Refer to Section [161.06](#).

If the restriction is axle weight or total weight, the sign should be installed to permit trucks to take an alternate route. This may require a supplemental plate advising truckers of the distance to or location of the restricted bridge. In some cases, it may be necessary to detour trucks off the state highway system to bypass a restricted bridge. Under these conditions, the truck detour signing should be coordinated with local officials and standard detour signing specifically for trucks should be installed throughout the detour roadway.

167.05 Low Clearance Signs



The "LOW CLEARANCE" (W12-3) sign should be mounted on overhead structures with a clearance of 16 feet (4.88 m) or less.

The "LOW CLEARANCE" (W12-2) sign should be used in advance of low bridges, underpasses, and other overhead structures with a clearance of 16 feet (4.88 m) or less where the sight distance to the overhead structure is restricted. This sign may be used in conjunction with the clearance signs (W12-3) mounted on the structure.

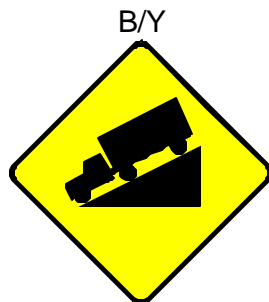
For ITD maintained bridges where under clearance posting is required on the local highway system, ITD must share responsibility for the installation and maintenance of the “Low Clearance” signs with the local jurisdiction for all structures on the State system. Maintenance agreements with local jurisdictions are encouraged, with ITD furnishing the signs and the local jurisdiction providing the labor. However, ITD has the ultimate responsibility for sign installation and maintenance. All “LOW CLEARANCE” signs on local roads shall be inventoried and routinely inspected.

Clearances shown on the signs should be approximately 3 inches less than actual. If seal coats or surface overlays decrease the clearance, the clearances should be re-measured, the signs changed if necessary, and a report sent to the Permits Section.

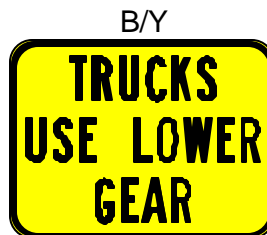
Measure clearances in accordance with the following procedures:

- For SAG verticals, the vertical measurement shall be taken from the center of a 50-foot (15 m) chord by subtracting the middle ordinate measurement from the total measured clearance for each approach.
- Measure the clearance at the point that gives the minimum clearance between the overhead structure and the travel way of the roadway. For safety, the travel way should be considered 4 feet (1.2 m) plus/minus outside the shoulder stripes.
- Make sure the measurement is taken from the lowest beam of the structure.

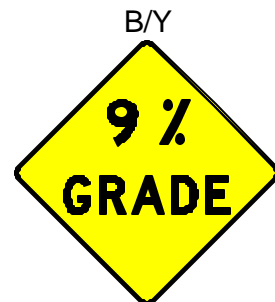
SECTION 168.00 - HILL AND GRADE SIGNS



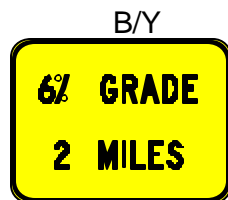
W7-1



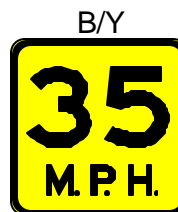
W7-2



W7-3



W7-4



W13-1



W14-4

168.01 General. “Hill” and “Grade” signs, as illustrated in Figure [168.01-01](#), are recommended on descending grades under the following conditions:

- 5% grade and more than 5 miles (8 km) in length
- 6% grade and more than 4 miles (6.5 km) in length
- 7% grade and more than 2 miles (3 km) in length
- 8% grade or greater and more than 1 mile (1.5 km) in length

Downgrades that are less than these lengths for each respective percent of grade should also be preceded by the installation of “Hill” and “Grade” signs if any part of the grade is on a horizontal curve with radius greater than 4 degrees curvature (less than 450 m).

The 48-inch (1219 mm) “Hill” and “Grade” signs should be used if accident experience or field observation indicates the need for more emphasis.

Typical signing and delineation for truck escape ramps are illustrated in Figures [168.01-02](#), and [168.01-03](#).

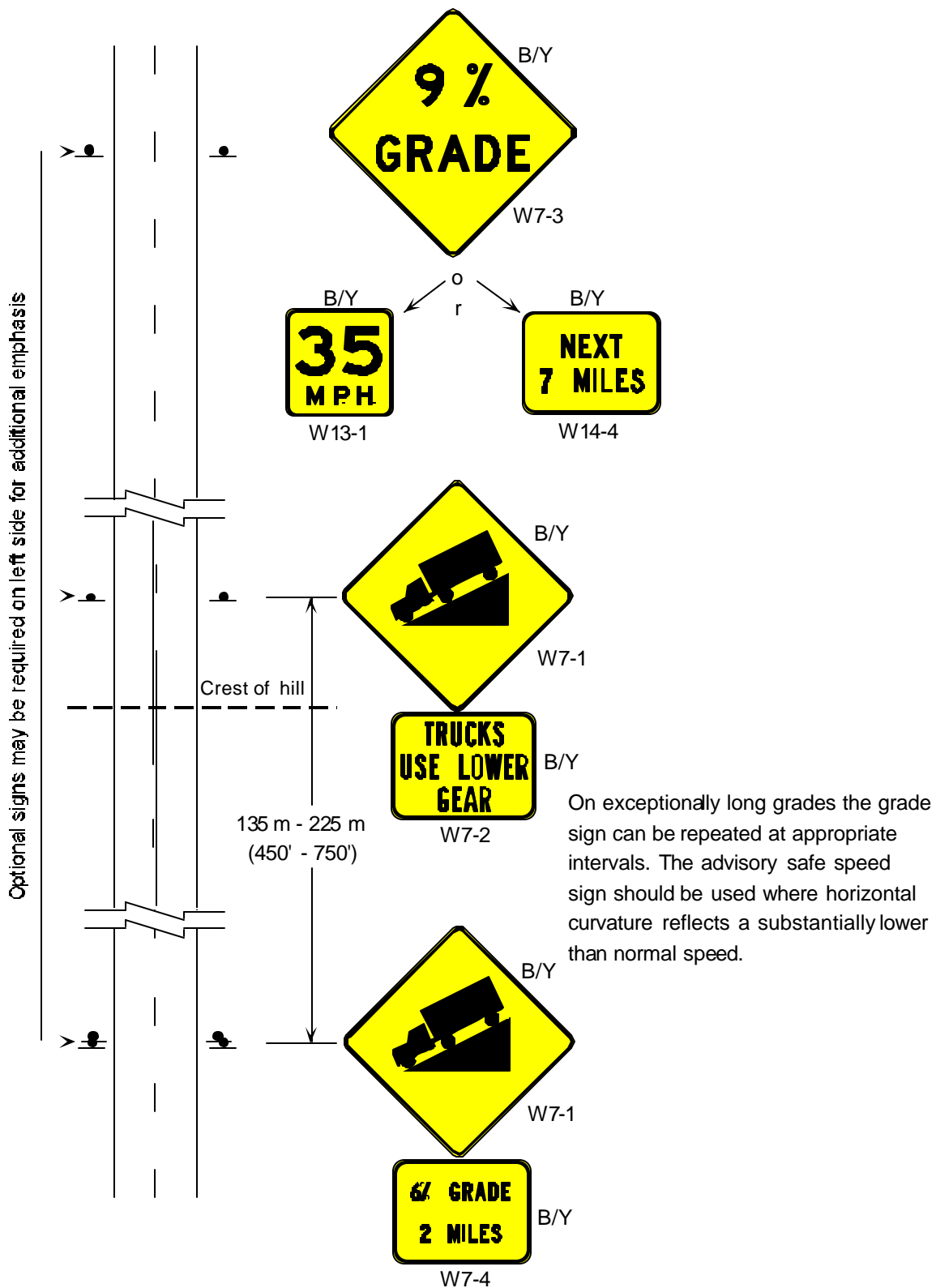


Figure 168.01-01 Typical Descending Hill And Grade Signing

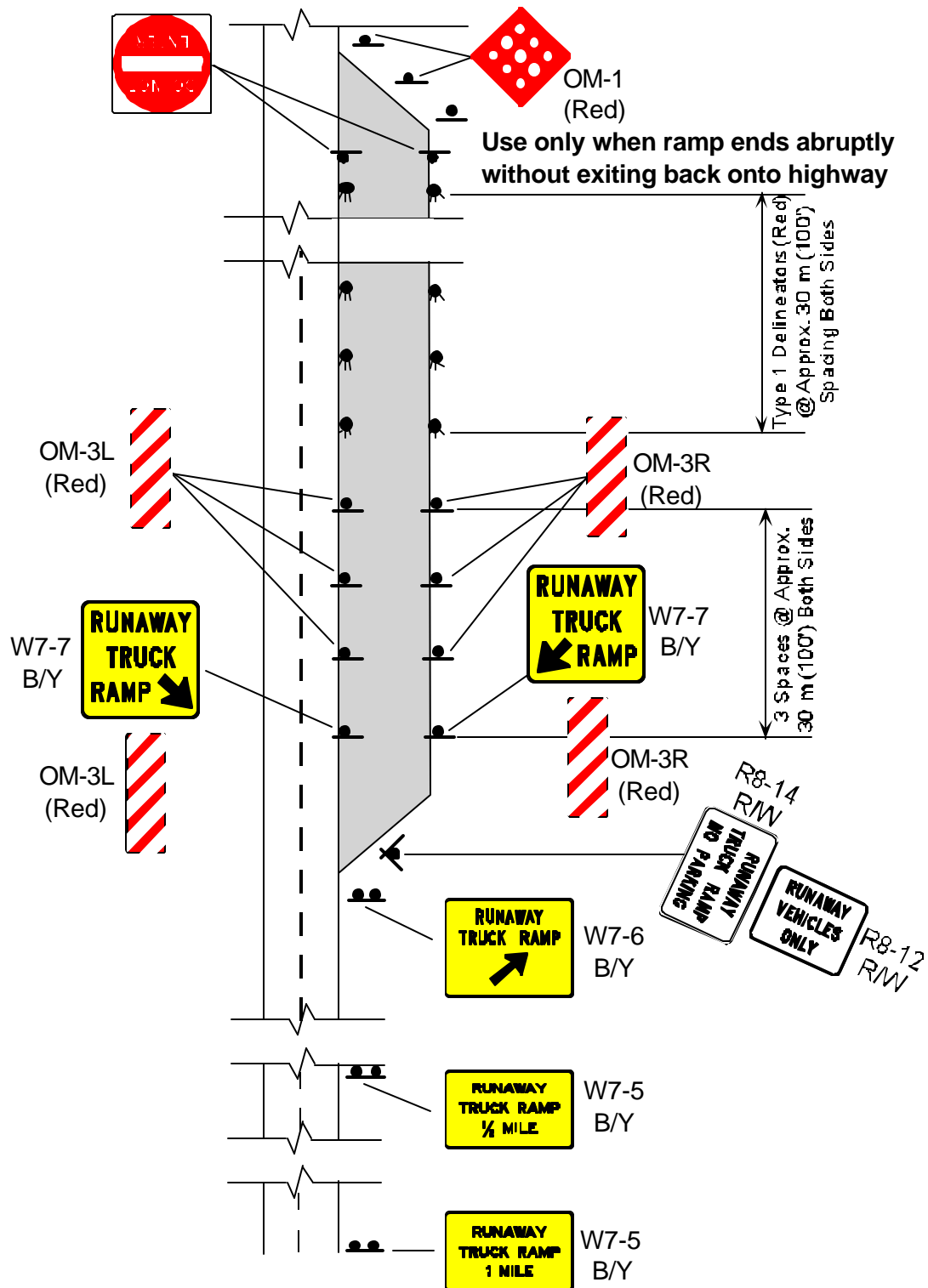


Figure 168.01-02

Typical Signing And Delineation For Runaway Truck Escape Ramp With Ramp Parallel To Roadway

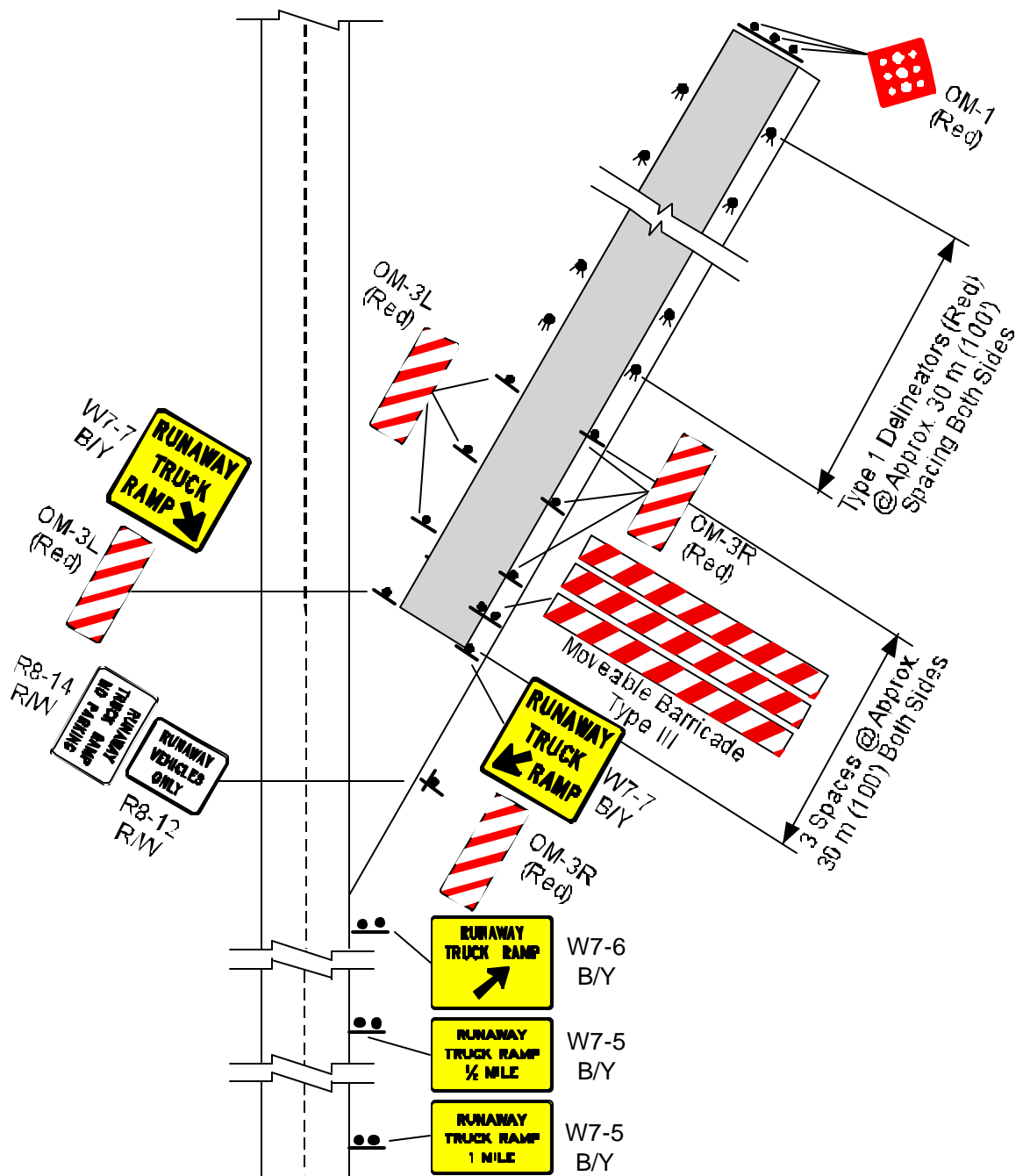
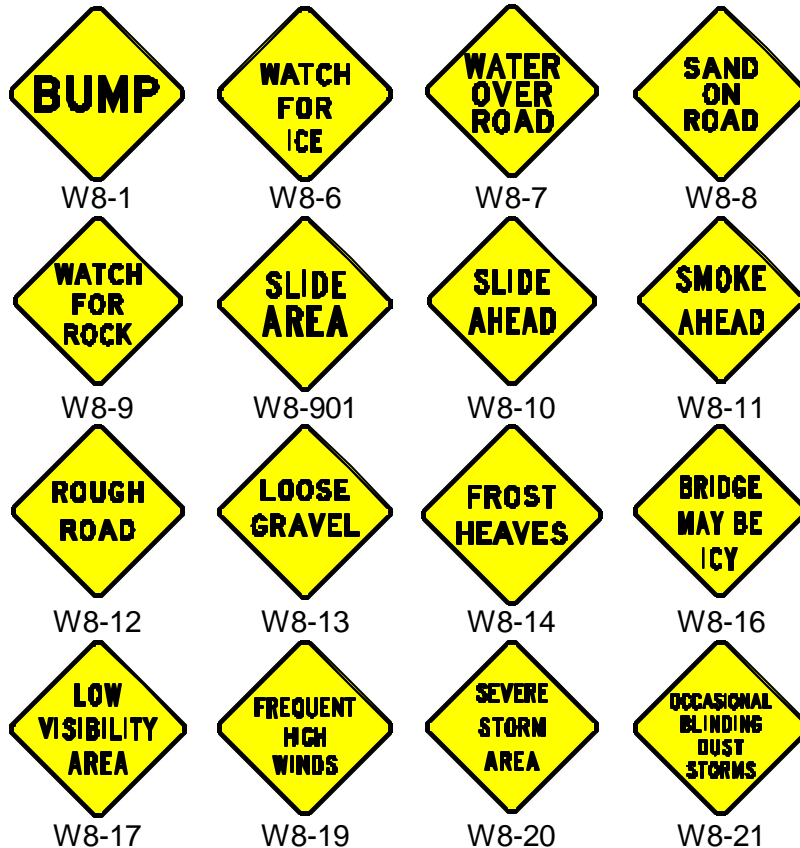


Figure 168.01-03

Typical Signing And Delineation For Runaway Truck Escape Ramp With Ramp Diverging From Roadway

SECTION 169.00 - PAVEMENT AND ROADWAY CONDITION SIGNS

ALL SIGNS ARE B/Y



169.01 General. Pavement and roadway condition signs are designed to warn motorists of temporary conditions. Install these signs promptly when needed and remove or hinge them as soon as the condition has been corrected. If these signs are not properly managed, their effectiveness is seriously reduced. Where a specific condition occurs for several miles, it is desirable to repeat the signs at suitable intervals. An auxiliary mileage sign (W14-4) also may be used in conjunction with these signs.

Note: Use orange background only when there is maintenance and construction activity.

SECTION 170.00 - MISCELLANEOUS CROSSING SIGNS

ALL SIGNS ARE B/Y



W11-1



W11-201



W11-301



W11-4



W11-5



W11-6



W11-7



W11-17

170.01 General. These miscellaneous advanced crossing signs may be installed at locations requiring their use. Remove them when the crossing condition no longer exists. These signs will be reflectorized unless for daytime use only.

If short-term, intermittent farm operations require signing, arrangements shall be coordinated with the farmer to provide signs on portable stands that can be utilized during the short period of time the highway is restricted.

Crossing signs may be used to supplement advance crossing signs as a means of assisting the vehicle operator in defining the specific point of crossing. Such signs should be used only at locations that are unusually hazardous or at locations not readily apparent. When used, the crossing sign should be located immediately adjacent to the crossing location. These signs are distinguished from advance crossing signs by the addition of crossing lines on the symbol plate.

Fluorescent Yellow-Green is an acceptable optional color for use on Pedestrian, Bicycle, and School warning signs. Sign colors should not be mixed at a selected site area.

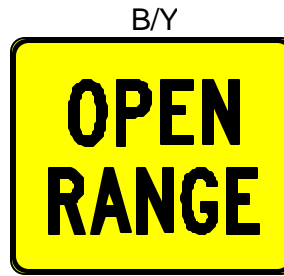
Game crossing signs may display a message that is either pictorial or text. Text messages may be beneficial in those areas prone to sign vandalism.

SECTION 171.00 - MISCELLANEOUS WARNING SIGNS

171.01 Livestock Signs



W11-9

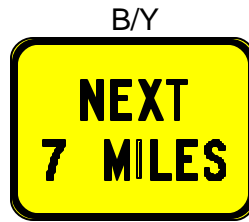


W11-901

Along many rural sections of the state highway system, the land adjacent to the highway is unfenced and designated as "OPEN RANGE." "OPEN RANGE" encompasses all unenclosed lands outside cities and herd districts upon which cattle by custom, license, lease or permit are grazed or permitted to roam. The "open range" designation imposes no duty on the livestock owner to keep his animals off the highway and grants the livestock owner liability immunity in case of a vehicle-animal collision.

The "WATCH FOR STOCK" (W11-9) or "OPEN RANGE" (W11-901) sign may be installed in areas of unfenced "open range" grazing land where livestock are along or frequently crossing the highway. The sign should be installed at 5-mile (8 km) intervals to emphasize the potential hazard associated with roaming livestock.

171.02 "NEXT ____ MILES" Auxiliary Sign



W14-4

Use the "NEXT ____ MILES" sign with other warning signs when the conditions shown by the warning sign exist over for 2 miles (3.2 km) or more on a section of the highway. Mount this sign as an auxiliary plate directly under the warning sign on the same post.

171.03 “SLOW TO _____” Warning Sign



Use the “SLOW TO _____” sign to provide the motorist with an advisory operating speed through a hazardous area. This sign should be used in conjunction with other appropriate warning signs. The speed shown on the sign is an advisory speed and not an enforceable limit. It is very important that the indicated advisory speed be realistic for the roadway conditions. A speed indication that is too low will be ignored and breeds disrespect for all such signs. Locations where this sign may be applicable are the beginning of diversion detours, roadway transitions or other roadway features that require reduction in normal vehicle operating speeds for motorist safety.

171.04 “BE PREPARED TO STOP” Sign



The “BE PREPARED TO STOP” sign normally is used in conjunction with flagger signs (W20-7) as optional construction and maintenance zone warnings for the motorist.

171.05 Impaired Pedestrian Sign

ALL B/Y



W11-18A

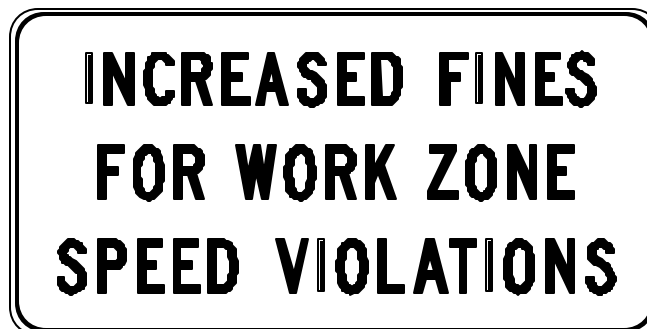
Signs to warn road users of visually or hearing impaired pedestrians may be installed at or near crosswalks according to the following guidelines:

- Confirm that the crosswalk is being used by impaired pedestrians. This should include:
 - A written physician's statement documenting the impairment
 - Verification of the pedestrian's residence or need to utilize a specific crosswalk.
- The word "child" and "pedestrian" as used in these signs are interchangeable and should be matched to the situation.
- Establish a three-year follow-up procedure to monitor continued need for sign placement. The signs shall be removed if it is verified that the crosswalk is no longer used by impaired pedestrians.

SECTION 172.00 - WORK ZONE SIGNS

172.01 "INCREASED FINES FOR WORK ZONE SPEED VIOLATIONS"

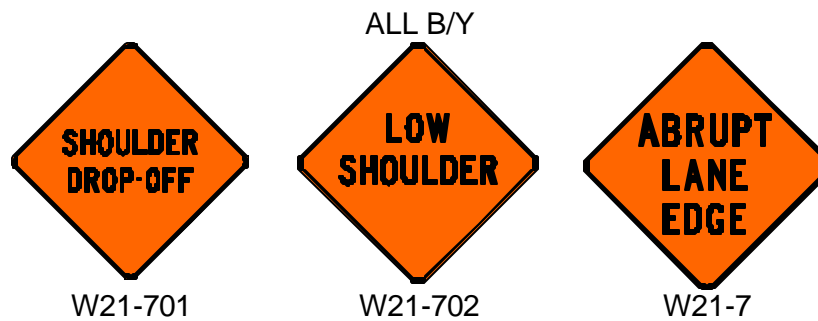
B/W



R2-6 (P)
R2-6A (I)

The “INCREASED FINES...” (R2-6 and R2-6A) sign is to be used in construction zones where the regulatory speed limit is reduced from the normally posted speed limit and the speed limit reduction refers to that portion of the construction zone where work interfering with through traffic is in progress. Refer to Section [601.01.01](#) for information on usage and placement. Placement of an “Increased Fines” sign must be followed by the placement of a “REDUCED SPEED AHEAD” sign and the appropriate “SPEED LIMIT __” sign.

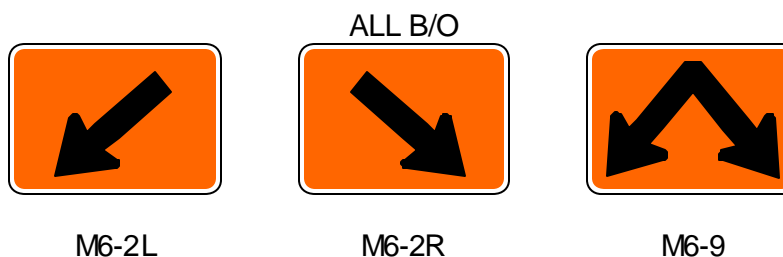
172.02 Abrupt Edge Signs



Pursuant to an FHWA directive dated December 22, 1997, the “Abrupt Edge” signs displaying an abrupt edge symbol are no longer authorized for use on Idaho’s highways. The “SHOULDER DROP-OFF” (W21-701) sign shall be used when the shoulder drop-off exceeds 3 inches (76.2 mm) and is not protected by a portable barrier. The “LOW SHOULDER” (W21-702) signs shall be used when shoulder drop-offs are less than 3 inches (76.2 mm).

The “ABUPT LANE EDGE” (W21-7) sign shall be used to denote uneven roadway surfaces between lanes.

172.03 Signs Mounted On Drums in Work Zones



Signs may be mounted only on closed-top drums in work zones where the posted work zone speed is 35 mph or less. Signs shall be either M6-2L/R or M6-9. See Section [601.03.01](#) for mounting specifications.

SECTION 173.00 - MISCELLANEOUS MAINTENANCE SIGNS

173.01 "SNOW REMOVAL EQUIPMENT" Sign

B/O



W21-16

The "SNOW REMOVAL EQUIPMENT" (W21-16) sign shall be hinged to display only during snow removal months.

173.02 "BREAKUP LIMITS" Signs

ALL B/O



R16-5



R16-6

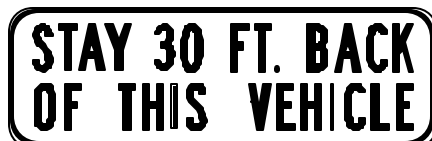


R16-4

Section [49-1005](#), Idaho Code, provides authority whereby the Idaho Transportation Board may reduce allowable weight or size or permissible speeds of vehicles traveling on state highways under certain conditions. Posting of all "BREAKUP LIMITS" signs shall comply with Rule [39.03.14](#), Rules Governing Policy During Spring Breakup Season in cooperation with the Port of Entry. Details for sign placement are contained in Section [340.00 of the Maintenance Manual](#).

173.03 Signs Mounted On Back Of Vehicles

R/W



R18-1

R/B/W



R18-2

Mount signs, when appropriate, to warn motorists of a safe following distance behind maintenance equipment.

174.00 Reserved.

SECTION 175.00 - ROUTE MARKERS

175.01 County Route Markers

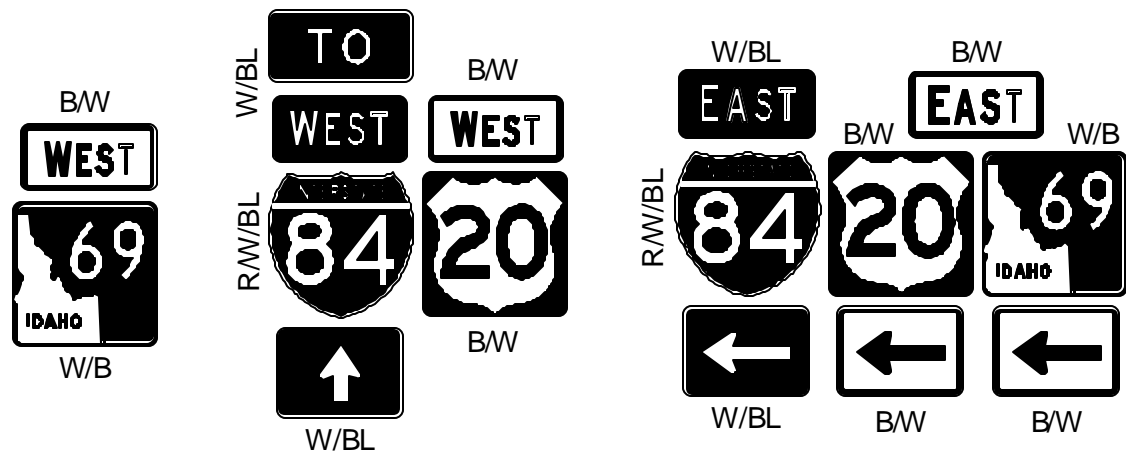


County route marker assemblies consisting of a route marker, directional arrow and destination sign may be installed on the state highway system where approved county routes intersect a state highway under the following conditions:

- The county route must be approved by the Route Numbering Committee of the Idaho Association of Commissioners and Clerks.
- The county route shall have all signing installed in accordance with the MUTCD prior to the installation of county route markers on the state highway system.
- All traffic-control devices along the county route shall be kept current with the MUTCD or the county route marker assemblies on the state highway system may be removed.

175.02 Highway Numbering System. The legal authority for the numbering of state highways is established by Administrative Policy [A-09-03](#) and Board Policy [B-09-03](#). Also refer to MUTCD section 2D-48, Signing of Named Highways. Requests to rename highways, bridges and other structures require legislative approval. (See Section [177.02](#))

175.03 Route Marker Assemblies



The use of cardinal direction markers are defined in the MUTCD. To clarify their usage when used with route marker assemblies on the interstate with an overlapping U.S. or S.H. routes the above-pictured examples shall apply.

Install the white-on-blue cardinal plate above the interstate route marker. A single cardinal of matching color may be used above the U.S. and S.H. route marker combinations.

175.04 Confirming Route Marker Assemblies. Confirming route marker assemblies should be installed at 10 mile (16 km) intervals between interstate interchanges and at 5 mile (8 km) intervals between junctions or cities in rural areas on other highways.

175.05 Designation of Community Connections. Where an interstate highway passes in the immediate vicinity of a community, state route numbering for street and highway connections will be accomplished in the following manner:

- When a U.S. or state numbered highway overlaps the interstate on each approach to a community and a section of road which is a part of the state highway system leaves the interstate, passes through the community and rejoins the interstate on the opposite side of the community, the route through the community may be designated as a business route of the overlapping U.S. or state highway. Carry the assigned U.S. or state highway route number past the community as an overlapping portion of the interstate highway.

Alternately sign the route through the community as an interstate business loop with the concurrence of the Transportation Board.

- When a U.S. or state numbered highway interchanges with an interstate highway in the vicinity of a community, carry the assigned route number only on the intersected route. In addition, use interstate trailblazer assemblies to guide motorists outbound from the community center to the interstate highway.

- When an interstate highway interchanges with a road or street in the vicinity of a community and such road or street is not a part of the state highway system, the governmental unit responsible for such road or street may, subject to approval by the Transportation Department, and at its own option and cost, use trailblazer assemblies to guide motorists outbound from the community center to the interstate highway via the connecting road or street.

175.06 Forest Service Road Junctions

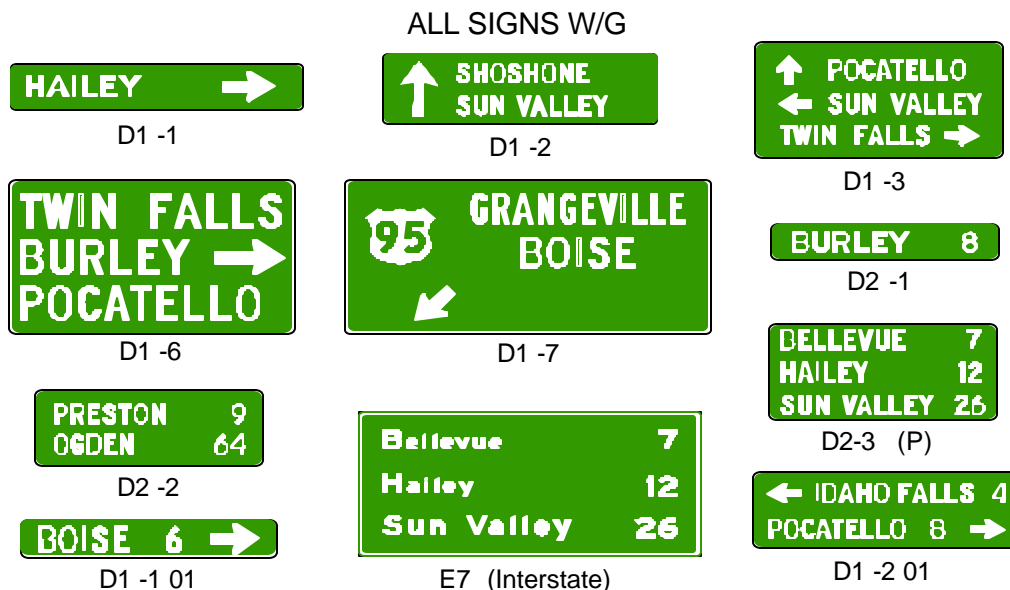


Forest Service route marker assemblies consisting of a route marker, directional arrow and destination sign may be installed by the Department on the state highway system where important Forest Service routes intersect a state highway.

The Forest Service route shall have adequate route number markers installed on the particular route prior to the installation of Forest Service route marker assemblies on the state highway system.

Advance turn assemblies on the state highway system to the Forest Service route are not normally required.

SECTION 176.00 - DESTINATION AND MILEAGE SIGNS



176.01 General. All destination and mileage signs on state highways and interstate interchange ramp terminals shall have green reflectorized background with reflectorized white lettering. Destination and mileage signs on interstate highways shall have a green reflectorized background with reflectorized white legends. Use upper and lower case text copy for interstate signs.

All destination and mileage signs shall be approved by the State Traffic Engineer prior to their installation.

176.02 Destination Signs. Do not use more than three names on a combination of ground-mounted destination signs. Show at least one destination for each leg of a junction. For a four-way junction, this would be one destination ahead, one left and one right. For three-leg junctions, a second city may be shown on one of the legs. The MUTCD does permit the option of four destination names under specific circumstances. The approval of four-destination usage shall be made by the State Traffic Engineer, who will consider data justifying motorist need for a fourth destination and the MUTCD provisions.

The destinations indicated will usually be the next city with services available. If more than one destination is used, the second destination should be a control city, provided the control city is on the same route.

Overhead signs shall have no more than two destinations shown on each sign mounted on the same overhead sign structure.

Destination signs may be installed directing traffic to destinations reached by unnumbered routes only if the road from the highway to the destination is well marked and has an all-weather surface. A maintenance agreement with the local highway jurisdiction is advised to ensure pavement and signing integrity of the route.

Major destinations and control cities on the state highway system within Idaho and bordering states are shown in Figures [176.02-01](#) and [176.02-02](#).

176.03 Location of Destination Signs. For “T” type intersections where all approaching traffic must stop on one approach, the destination sign may also be located opposite the approach for driver convenience and readability.

176.04 Mileage Signs. The mileage sign can be one, two, or three names, depending on the particular motorist requirements for the route being signed. It is not necessary to use two or three names on mileage signs if the names are not needed.

Do not show more than three names on a ground-mounted mileage sign. The top name shall be the next destination with services available. The second name should be the next major city or attraction. The third name, if used, should be the control city. All destinations shown shall be on or near the route being signed.

In all cases, signing should be for the best interest of the traveling public. Signs should not be installed to increase the prestige of a community.

The selection of names for mileage signs shall follow the MUTCD criteria. In addition, it is desirable that the top name include a place that provides motorist services. The middle name should include an intermediate destination of general importance to the motorist. The third, or bottom name, shall be the major route destination or control city indicated on route junction destination signing. The major destination shall be shown on all mileage signs until that destination is reached. The State Traffic Engineer shall approve all names used on mileage signs.

176.05 Location Of Mileage Signs. In addition to locations specified by the MUTCD, mileage signs should be installed on routes leaving incorporated communities, highway junctions, state lines and places of significant traffic generation. Closely spaced application of mileage signs within a few miles of each other should be avoided. An intermediate sign or signs may be installed where a gap of 20 miles (32 km) or more exists between mileage signs using the above basic criteria. Compute the mileages shown on the sign to the city limits.

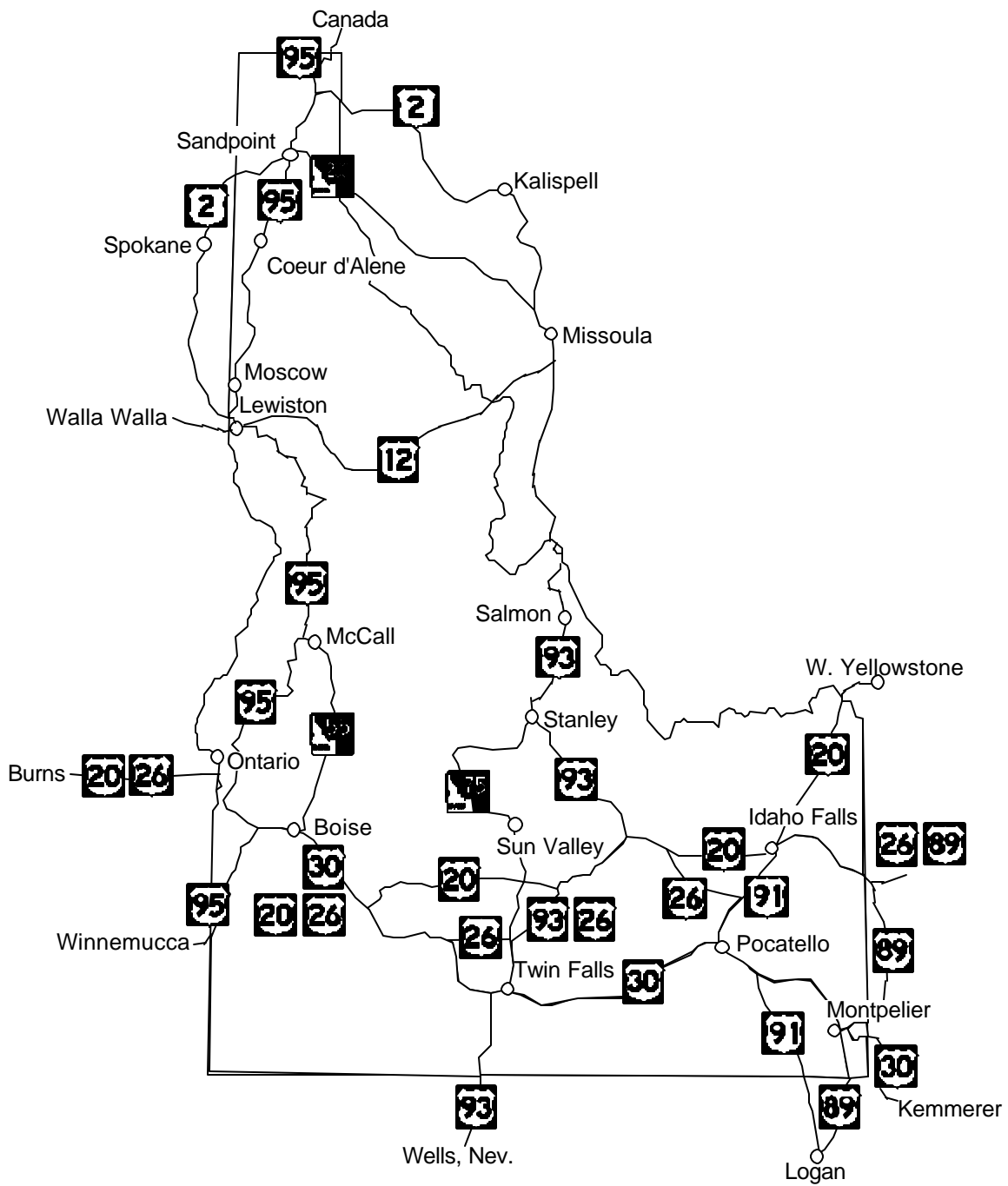


Figure 176.02-01 Major Destinations On The State Highway System

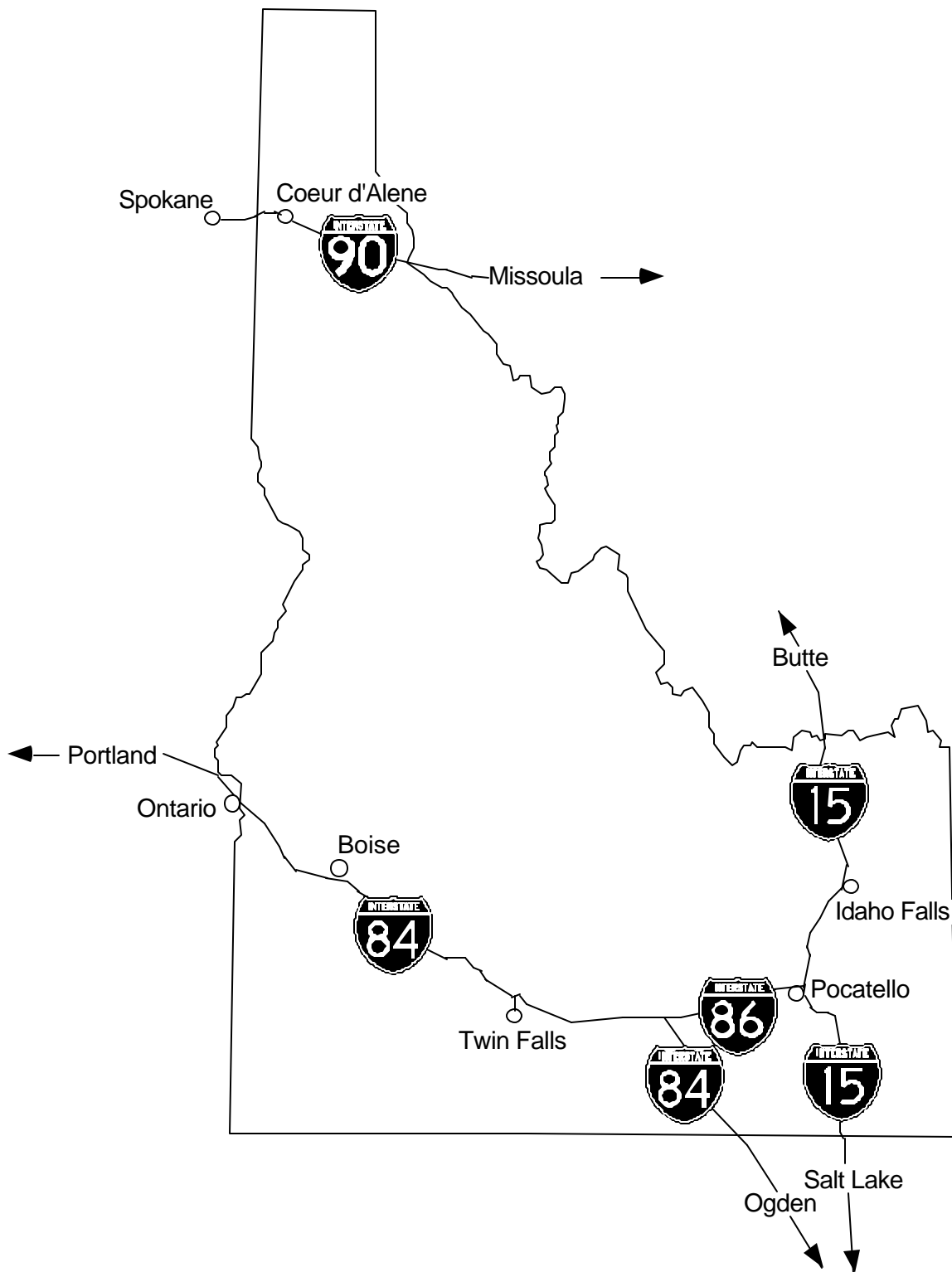


Figure 176.02-02 Control Cities On The Interstate Highway System In Idaho

176.06 Interstate Supplemental Guide Signs. “Supplemental Guide Sign” means any destination sign that is added to the standard sequence of interchange signs, as required in the MUTCD, showing places accessible from an interchange other than those named on the required interchange guide signs.

The MUTCD permits the installation of supplemental guide signs between the major interchanges on interstate and other fully controlled access highways when there is a need to sign additional destination communities, major motorist attractions or specific traffic generators. However, supplemental guide signs shall be used sparingly to reduce the motorist message overload on the interstate, minimize sign messages on interchange approaches and reduce the signing required on interchange ramps and ramp terminals.

Supplemental guide signs and place names shall be approved based on their significance or service to motorists to the facility to be signed, and then only where sufficient longitudinal space exists to accommodate the sign without interfering or conflicting with the required guide signs or other necessary traffic control devices.

The following general rules shall apply to interstate supplemental guide signs:

- Supplemental guide signs shall be approved by the State Traffic Engineer.
- One supplemental guide sign on each interchange approach.
- No supplemental guide signs at freeway-to-freeway interchanges.
- Two destination names are permitted on each supplemental guide sign. If more than two names are requested, the State Traffic Engineer shall determine the two that have the greatest motorist need.
- No more than one supplemental guide sign for each direction for any destination or traffic generator.
- Sign shall usually be located at the exit nearest the facility or for the most direct route to the facility.
- White legends on brown background may be used for park, recreational or scenic facilities.
- In accordance with state statutes, no commercial advertising shall be permitted on any supplemental guide sign.
- Seasonal supplemental guide signs are permissible if arrangements are made to lower, remove or otherwise delete the supplemental message on all signs, including trailblazers during the off-season.
- All supplemental guide signs on the interstate shall require ramp-terminal signing and trailblazer signs, if necessary, along the intersecting route from the interstate to the facility.
- The interstate sign shall not be installed until all trailblazer signs are in place.

- All additional guide information in excess of the two destinations used on the advance guide and exit direction signs shall be considered supplemental guide sign legends.

The district should review existing supplemental guide signs and requests for additional supplemental guide destinations with the district's recommendation made to the State Traffic Engineer. The recommendation should include location relative to interstate interchanges, traffic generation data (e.g., enrollment, employment daily visitors, etc.) and proposed signing plan. If a supplemental destination requires removal of an existing supplemental name, the district should support its recommendation by listing the basis for deleting an existing signed destination.

The following traffic generators may be signed regardless of the traffic volumes generated or distance, provided a significant motorist need can be justified:

- Other incorporated cities
- Federal and state parks
- Major recreational areas

The following are minimum criteria to warrant consideration of interstate supplemental guide signs for the listed facilities:

<u>Types of Generators</u>	<u>Criteria</u>	<u>Urban Area</u>	<u>Rural Area</u>
Airports	Number of regularly scheduled commercial departures and	25/day	15/day
	Maximum distance from interchange	5 miles (8 km)	5 miles (8 km)
Colleges & Universities, including satellite facilities	Total enrollment full- and part-time students and	4,000	2,500
	Maximum distance from interchange	5 miles	5 miles
Military Bases	Number of employees and permanently assigned personnel and	5,000	5,000
	Maximum distance from interchange	7 miles	7 miles
Arenas/Auditoriums/ Convention Facilities/ Stadiums	Annual attendance	170,000	130,000
	and	3,000	2,000
	Seating capacity	5 miles	5 miles
Ski Areas/Amusement Parks/ Fairgrounds	and		
	Maximum distance from interchange	15 miles	15 miles
Historical Sites/National Monuments & Parks		25 miles	25 miles

Supplemental guide signs will not be considered for the following traffic generators except for special conditions or unusual circumstances that must be fully justified:

Businesses

TV/radio stations
Theaters
Motels/hotels/inns*
Trailer parks*
Industrial parks and plants
Shopping centers

Cemeteries

Local or state
Private/public
Military

Communities

Civic centers
Libraries
Churches
Subdivisions

Governmental

Research/experimental
County and city police facilities
Courthouses
Driver's license centers
Highway buildings
Jails/prisons
Civil Defense facilities
Maintenance facilities
Power plants
Ranger stations
USFS

Schools

Grade/high
Vocational/trade
Seminaries
Private

Historical

Homes and buildings
Privately owned facilities

Medical

Mental facilities
Research facilities
Sanitariums
Infirmaries or treatment centers
Veterans facilities
County, fraternal or nursing homes
Retirement facilities
Humane facilities
Emergency medical service*

Military

Sites or detachments
Armories
Arsenals

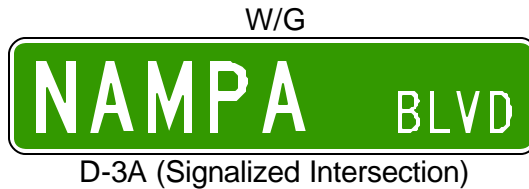
Recreational/Conservational

Country clubs and golf courses
Fish hatcheries
Game farms, preserves and refuges
Tree Nurseries/Arboretums
Scout, church, 4-H, youth, and
YMCA/YWCA camps

*Items may be included on motorist service signs (gas-food-lodging-hospital-camping)

SECTION 177.00 - STREET, ROAD AND BRIDGE NAME SIGNS

177.01 Signalized Intersection Signs



Street name signs should be installed on all approaches to signalized intersections. On most signal mast arms, the sign should be mounted a minimum of 2 feet (0.6 m) to the right of the signal head. On mast arms with two or more heads, the sign should be mounted a minimum of 2 feet (0.6 m) to the right of the right-hand head. With span wire mounting of the signals, the signs may be mounted on the signal pole above the signal head. The street name shall appear in all capitals of at least 10-inch (250 mm) series “B” or “C” letters. Abbreviations such as “ST,” “AVE,” “BLVD,” “N” or “DR” may be reduced to not less than 6-inch (150 mm) series “B” or “C” capital letters.

177.02 County Road and City Street Name Signs



ITD will permit county road and street name signing, including naming of state highway segments, on highway rights-of-way subject to the following provisions:

- The signs, except at major intersections, shall be installed and maintained by the county, city or highway district.
- Signs installed on the highway right-of-way shall be covered by means of an approved agreement with the county, city or highway district.
- The signs shall be in conformance with the MUTCD. It is recommended, however, that the signs be a retroreflectorized white message on a retroreflectorized green background with 6-inch (150 mm) lettering. The words, or abbreviations for, “street,” “avenue,” “road,” etc., may be in smaller letters.
- Street or road names and designations (such as Drive, Avenue, Way) shall be consistent to facilitate accurate accident reporting. Name changes at city or county lines and the use of nicknames and cardinal directions is discouraged.
- Signs shall not be installed at locations that interfere with other official traffic-control devices.
- The signs may be mounted above stop or yield signs on the same post, on light poles or on separate posts. When mounted on a post above a stop or yield sign, an approved post adaptor shall be used.

- The desirable location for the signs is on the far right corner of the intersection for traffic on the highway.
- The Department may, after due written notice to the county, city or highway district, remove signs from the state highway right-of-way that are not properly maintained.

Larger road-name signs may be installed by ITD in high-speed rural areas where traffic volumes using the county road are high and supplemental advance signing is required to identify their location. It is not intended that the Department will sign a large number of the county road intersections, but only those of major importance. At obscured county road intersections, intersection warning signs should be installed. It is desirable that a county road name plaque be installed below the warning signs to provide the motorist advance information on the county road name.

177.03 Grade Separation Road Name Signs



These signs may be installed on grade separation structures where the crossroads are named and have local location value. They should have limited use.

177.04 Private Road Name Signs. Street and road name signs at intersections of private roads and state highways are subject to the same provisions as at intersections of public roads except that the street name signs for private roads should have retroreflectorized white letters on a retroreflectorized blue background and the signs may include the legend “Private Road,” “Private Lane,” etc., in small letters underneath the road name.

177.05 Special Road and Bridge Names. The Board determined on December 18, 1998, not to permit the designation of special road or bridge names on the State highway system. Legislative authority is required to designate special road or bridge names on the State Highway System (see Section [153.08.03](#)). Such signs shall be installed and maintained by the Department. Communities requesting special name designations shall sign for them off of the State right-of-way.

SECTION 178.00 - GENERAL SERVICE SYMBOL SIGNING FOR FREEWAYS

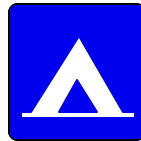
ALL SIGNS W/BL



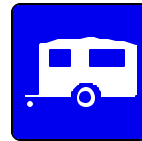
D9-1



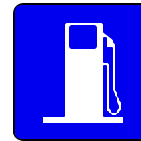
D9-2



D9-3



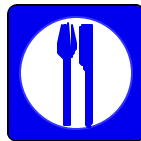
D9-4



D9-7



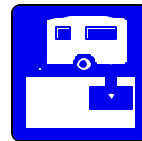
D9-8



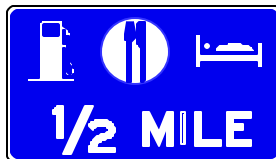
D9-9



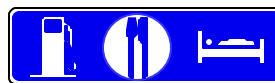
D9-11



D9-12



E10-1



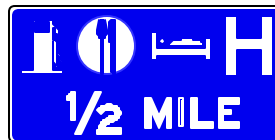
E10-2



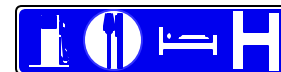
E10-3



E10-4



E10-101



E10-201

The “NEXT SERVICES __ MILES” (E10-4) sign may be used in conjunction with the applicable motorist service symbols on freeways where it has been determined by the State Traffic Engineer that signing for available services is critical to the safe travel of the motoring public due to the distance between available services. Gas is a required service in determining the distance for the E10-4.

Service symbol signs may be installed on freeways when the available service represented by the symbol is not visible from the freeway and is located within 3 miles (5 km) of the freeway, or in an adjacent bypassed community.

178.01 Rural Area Freeways. A general motorists service sign with symbols for hospital, gas, food, lodging and camping, next right (or a proper combination), depending on available services, may be installed on the freeway when the service is located in the immediate vicinity of the interchange or in an adjacent bypassed community which is serviced from the interchange.

If business loops are indicated at ramp terminals, no separate service signs are normally needed.

The symbol signs for gas, food, lodging, camping and trailer dump should not be installed for communities or locations where these services are apparent to the motorists or where logo signs for similar services exist.

When a general motorist services sign is used, symbol signs may also be used independently at the ramp terminals, directing motorists to the specific service when the direction is not apparent.

- “Immediate vicinity of the interchange” shall be interpreted as within one mile (1.5 km) of the freeway interchange.
- “Adjacent bypassed community” shall be interpreted as a community where motorist services are available, which is located within 1 mile (1.5 km) of the freeway and when the most direct driving distance from the freeway to the community and back to the freeway is 5 miles (8 km) or less.
- Hospital signing should be provided for a hospital within the community adjacent to and served by the freeway. Where more than one hospital is served by only one interchange, sign only to the hospital that has the shortest distance to it. Where a community is served by two interchanges of approximately equal distance to the hospital, sign for the hospital at the first interchange only. At ramp terminals, if the hospital symbol sign cannot be installed beneath the destination sign, the sign can be installed in advance of the ramp terminal on the back of the “WRONG WAY” sign as an alternate location. The name of the hospital shall not be used in indicating the direction to the nearest hospital. Additional hospital signing within the community is normally the city’s responsibility and should be coordinated with the city officials.

➤ ALL FACILITIES

No facility that restricts public use due to membership or subscription shall be approved for service symbol signing unless other non-restrictive facilities are present and operational at the proposed interchange.

➤ GAS

Motorist services shall include fuel, oil, water, and the following minimum requirements:

- (a) A maintained call list where full tire service is available.
- (b) Public services to include telephone, restrooms and drinking water.
- (c) Continuous operations, seven (7) days per week and sixteen (16) hours per day, or other minimums established by energy policy imposed by Federal or State authorities.

➤ FOOD

- (a) Approved and/or licensed by a State agency or political entity having jurisdiction.
- (b) Public services to include telephone and restrooms.
- (c) Minimum indoor seating for eight (8) persons.
- (d) Continuous operation to serve three (3) meals per day, seven (7) days per week. Minimum hours of continuous operation shall be 7 a.m. to 7 p.m.

➤ LODGING

- (a) Approved and/or licensed by a State agency or political entity having jurisdiction.
- (b) A minimum of four (4) rooms of hotel or motel space each with a private bath.
- (c) A telephone available for public use.

➤ CAMPING

Approved and/or licensed by a state agency or political entity having jurisdiction.

- (a) A minimum of fifteen (15) camping sites.
- (b) Adequate parking - at least one (1) parking space per campsite.
- (c) Public services to include telephone, restrooms, and drinking water.
- (d) Continuous operation seven (7) days per week and twenty-four (24) hours per day.

The signing should be covered or removed during the closed season if operated on a seasonal basis. Months of operation may be signed on a supplemental plaque placed below the camping sign if feasible. The cost of this should be billed to the owner of the facility if it is privately owned.

When there is a mixture of governmental and private campgrounds in the same direction and general area, install only one directional camping sign directing motorists to that general area.

178.02 Urban Area Freeways. A service sign with the proper symbol combination may be placed beneath the “Community Next Exit” sign on the freeway 2 miles (3 km) in advance of the first exit to the community where logo signs have not been installed for similar services at that exit. Trailblazers will not be installed by the Transportation Department directing motorists to the service facilities after exiting from the through route, with the exception of hospitals. Appropriate signing for the other services should be installed by the concerned governmental agency, private individual, or corporation, subject to the state laws governing advertising signing adjacent to the state highway system. All motorist service symbol signs shall meet the criteria of 178.01.

178.03 Signing Procedures. The District State Traffic Engineer will determine if the service to be signed is in the “immediate vicinity of the interchange” or in an adjacent, bypassed community, and whether the service meets the criteria of Section [178.01](#) above.

The District Traffic Engineer will request inspection and recommendations from the state Department of Health and Welfare on the eating, lodging and camping establishments. The District Traffic Engineer should receive concurrence from Health and Welfare before the appropriate symbols are installed.

The Department of Health and Welfare will inspect the establishment for health standards and for compliance to standards of rooms, bath facilities, hot meals, water supply and waste disposal.

Where several establishments for eating and/or lodging are available, and it can be reasonably assumed that the requirements of the above-stated policy are fulfilled, no inspection will be necessary.

Where available service is seasonal, the district will remove or cover the appropriate symbol on the service signs for periods of non-service. If the service facility has irregular hours/days of operation, the appropriate symbol will be removed. Where and when a campground is operated on a seasonal basis, signing shall apply only where the campground owner agrees to pay in advance for the Department’s cost of the seasonal installation and removal of the signing.

Forward a copy of each correspondence pertaining to service signing between the district and the Department of Health and Welfare to Headquarters Traffic.

Existing motorist service signs shall be removed at interchanges where logo signs are installed for that same service.

SECTION 179.00 - GENERAL SERVICE SYMBOL SIGNING FOR CONVENTIONAL HIGHWAYS

ALL SIGNS W/BL



D9-1



D9-2



D9-3



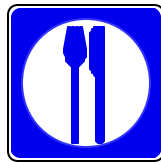
D9-4



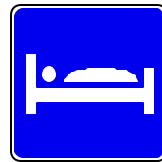
D9-7



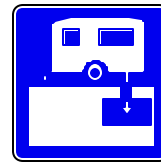
D9-8



D9-9



D9-11



D9-12



E10-4A

The “NEXT SERVICES __ MILES” (E10-4A) sign may be used in conjunction with the applicable motorist service symbols on highways where it has been determined by the State Traffic Engineer that signing for available services is critical to the safe travel of the motoring public due to the remoteness of the area, distance between services or the reasonable expectation that extreme hazards exist (i.e. avalanches, rock slides or floods). GAS is a required service to install the E10-4A.

179.01 Gas, Food, Lodging, Phone, Hospital and Camping Symbol Signs.

Service symbol signs may be installed on conventional highways on the State highway system under the following conditions:

- The available service represented by the symbol is not visible from the State highway, or is located within a bypassed community; and
- Is a fuel service facility located within a 3 mile (5 km) radius of the intersection; or
- Is a facility located within a 15 mile (24 km) radius of the intersection for all other services.
- Distance placards should be included for services signed beyond a 5 mile (8 km) limit.

- “Bypassed community” shall be interpreted as a community formerly on the State highway system and as a result of new highway alignment the available motorist services are not visible from the State highway system.
- When the driven distance from the State highway to a specific service facility is not more than 15 miles (24 km) in one direction (3 miles or 5 km maximum for gas facilities), special directional signs may be installed on the State highway system if the specific service facility meets the requirements of Section [183.04](#), Tourist Oriented Directional Signs (TODS).
- Hospital signing should be provided for a hospital within the community. Do not use the name of the hospital with the sign. Additional hospital signing within the community off the state highway system is the community’s responsibility and should be coordinated with the community officials.
- Gas, food, lodging and camping services must meet the following requirements in order to qualify for service symbols:

➤ GAS

Vehicular services shall include fuel, oil, water and meet the following minimum requirements:

- (a) Air a minimum of 50 psi (345 kPA).
- (b) A jack capable of lifting one (1) wheel of a three-quarter (3/4) ton pickup and lug wrenches with sizes to fit cars through pickup trucks.
- (c) A maintained call list where full tire service is available.
- (d) Public telephone, restrooms and drinking water.
- (e) Continuous operations, twelve (12) hours per day, and seven (7) days per week or other minimum(s) established by energy policy imposed by Federal and State authorities.

➤ FOOD

- (a) Approved and/or licensed by a State agency or political entity having jurisdiction.
- (b) Public telephone and restrooms.
- (c) Minimum indoor seating for eight (8) persons.
- (d) Continuous operation to serve three (3) meals per day, seven (7) days a week. Minimum hours of continuous operation shall be 7 a.m. to 7 p.m.

➤ LODGING

- (a) Approved and/or licensed by a State agency or political entity having jurisdiction.
- (b) A minimum of four (4) sleeping units complete with private bath facilities for each sleeping unit.
- (c) A telephone available for public use.

➤ CAMPING

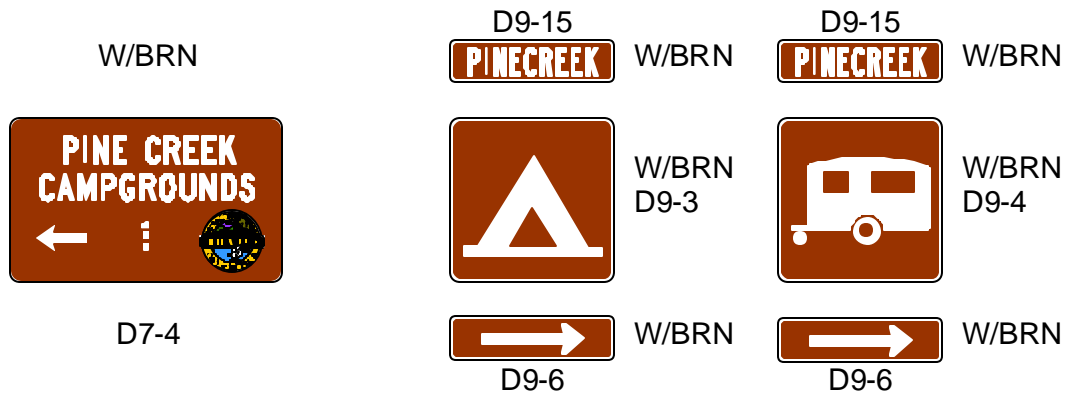
- (a) Approved and/or licensed by a State agency or political entity having jurisdiction.
- (b) Public telephone, restrooms and drinking water.
- (c) A minimum of twelve (12) camping and parking spaces reserved for overnight use.
- (d) Continuous operation seven (7) days per week and twenty-four (24) hours per day.

When directional signing is required off the state highway system to supplement service signing installed on the State highway system, the signing should be installed by the concerned governmental agency, private individual or corporation, subject to the state laws governing advertising signing adjacent to the state highway system.

All signing must be installed with the approval of the local jurisdiction and be in place before the service signing can be installed on the State highway system.

If directional signing is not maintained, the service signing will be removed from the State highway system.

179.02 Forest Area Camping Signs



Campgrounds may be identified by installing a name plaque in the same color above the camping symbol. The same rules apply as in other areas (Section [178.00](#)).

Service facilities adjacent to the highway and not apparent to motorists may be eligible for service symbol signs. It is not intended that these signs replace or supplement inadequate or improperly located private signs.

Where sight distance is limited, an advance sign assembly may be installed with the appropriate legends.

Forest signing shall be brown reflectorized background with white reflectorized lettering.

SECTION 180.00 - REST AREA SIGNS

180.01 Rest Area Signing

ALL W/BL



D5-1



D5-6



D5-2



D5-2A (I)

Install rest area signs as illustrated in Figures [180.01-01](#) and [180.01-02](#). Figure [180.01-01](#) illustrates typical signing for a “gateway” rest area. Gateway rest areas must have a staffed visitor information center on site. Figure [180.01-02](#) illustrates typical signing for a non-gateway rest area. Figure [182.02-01](#) illustrates typical signing for rest areas located at weigh stations.

Install auxiliary signs below the appropriate rest area signs denoting visitor or motorist information as well as historical or geological sites available at the rest area. Motorist information symbols, such as for telephone, may also be installed as auxiliary signs.

Areas to be signed as rest areas shall have complete services such as drinking water, rest rooms and tables, and shall be owned and operated by ITD.

180.02 “NEXT REST AREA” Sign

W/BL



D5-8

Install the “NEXT REST AREA” sign beneath the interstate “REST AREA NEXT RIGHT” sign when the distance to the next rest area exceeds 20 miles (32 km).

180.03 “REST AREA VISITOR INFORMATION CENTER” Signs

W/BL

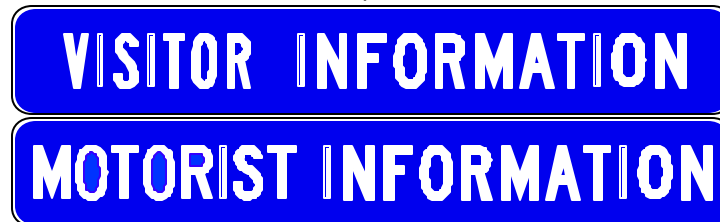


I-22

Install the “REST AREA VISITOR INFO CENTER __ MILES” prior to “Gateway” rest areas. Signs should be placed within 5 miles (8 km) of the rest area. Signs may be placed near the state line if warranted by rest area locations.

180.04 “VISITOR INFORMATION” Or “MOTORIST INFORMATION” Signs In Rest Areas

W/BL



I-51 or I-51A

Install the “VISITOR INFORMATION” sign below the “REST AREA 1 MILE” sign prior to “Gateway” rest areas.

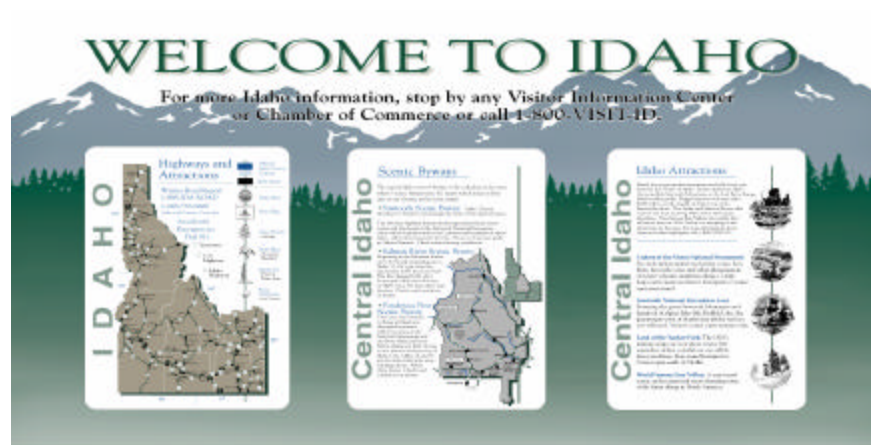
Install the “MOTORIST INFORMATION” sign below the “REST AREA 1 MILE” sign prior to non-gateway rest areas.

Install “Historical Site” or “Geological Site” signs, or motorist information symbols (W/BL), when applicable, below visitor or motorist information signs.

180.05 Rest Area Information Signs



I-19A



I-19B

Install the ITD Rest Area Information (I-19A) sign near the rest room facilities to provide motorist information.

A memorandum of understanding between the ITD and the Idaho Department of Commerce allows the Idaho Travel Council to provide visitor information signs (I-19B) in rest areas to promote tourism. Refer to this MOU for guidelines regarding sign maintenance and changes in visitor information.

180.06 Rest Area Pet Signs



Install the rest area pet signs when the rest area has a specially designated area for small pets.

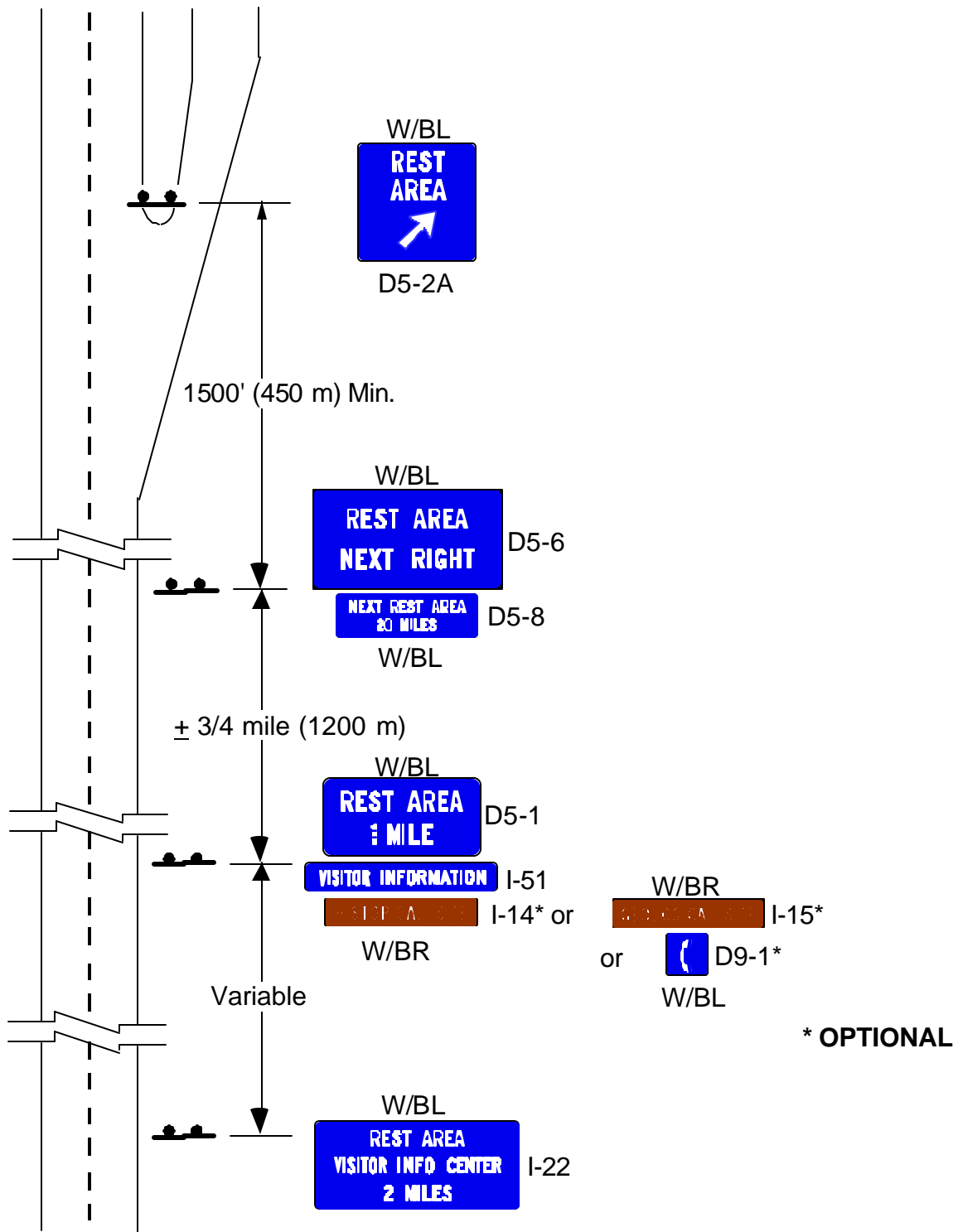
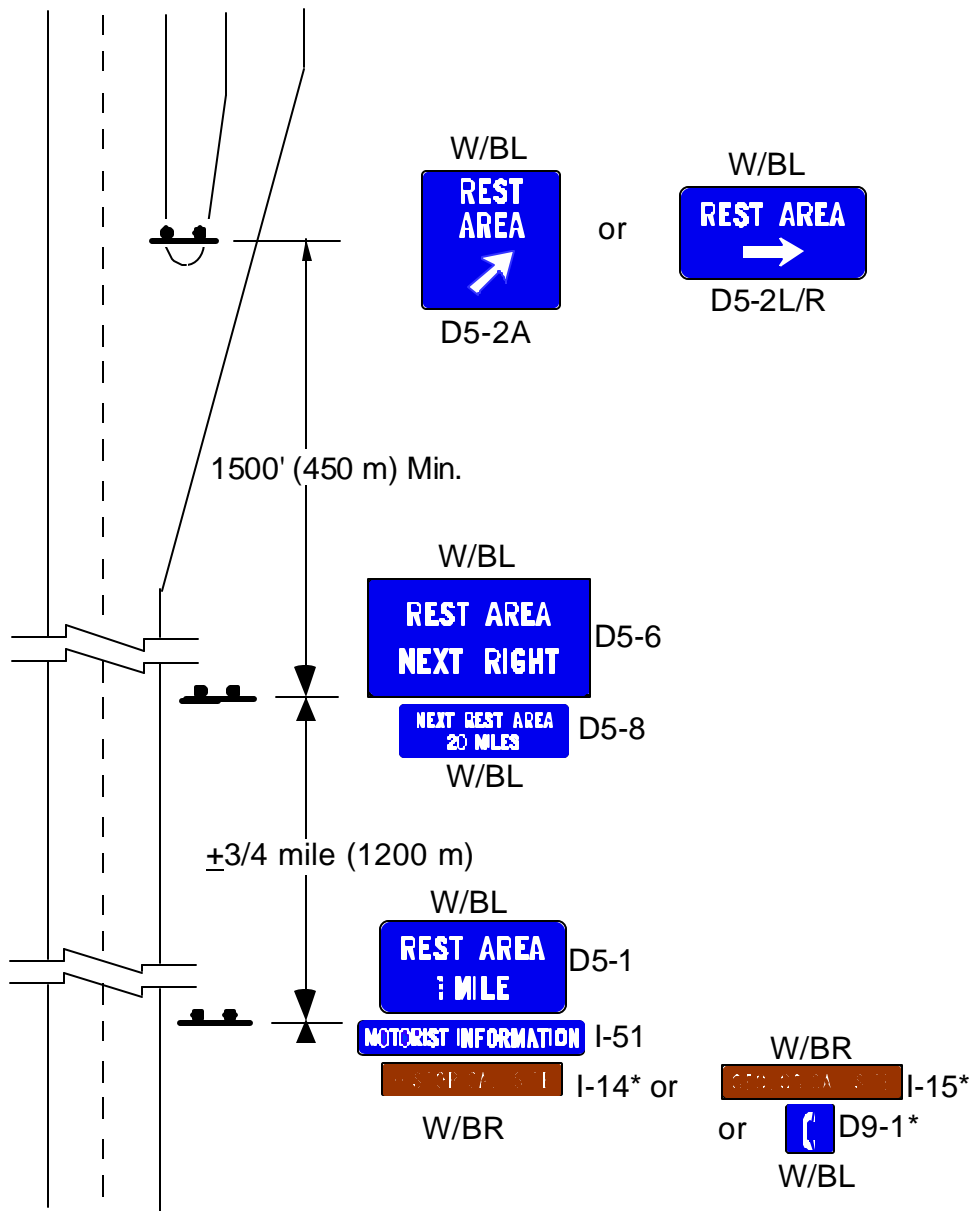


Figure 180.01-01 Gateway Rest Area Signing



*** OPTIONAL**

Figure 180.01-02 Non-Gateway Rest Area Signing

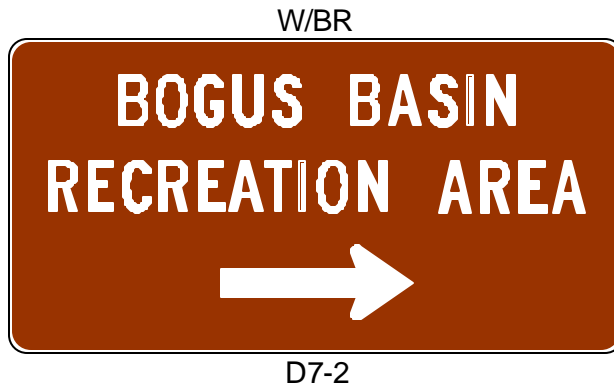
SECTION 181.00 - RECREATION AREA SIGNS

181.01 State Park Signs



State parks and campgrounds are under the jurisdiction of the Idaho Department of Parks and Recreation (IDPR). The state park and campground signs may be posted when the IDPR has requested signing for a designated state park or campground that requires signing from the state highway. The Department will install white on brown signs in advance of and at entrances to these areas or at the intersection of the primary road leading to them. If the designated areas are located off the state highway system, IDPR will be required to provide adequate signing on the access road to direct motorists from the highway to and from the site location. These off-system signs shall be purchased, installed, and maintained by IDPR. All off-system signing shall be in place prior to the installation of state highway signs.

181.02 Recreation Area Signs



Install brown on white recreation area signs for important multi-use recreational areas that generate significant volumes of traffic. These signs may be installed where the area is adjacent to the state highway system. Do not install these signs where the recreation area is over 32 km (20 miles) from the highway.

If an area is approved for signing, appropriate signing will be erected at the intersection of the access route or where the highway intersects the area boundaries. ITD will remove the signs if the access road is not properly maintained or the use of the area changes.

181.03 Sportsman Access Signs



Use the “SPORTSMAN ACCESS” (I-16) sign at locations selected by the Fish and Game Department in cooperation with the Traffic Section. Signing shall comply with the requirements of Section [153.01](#).

Sportsman accesses may be under the jurisdiction of the Idaho Department of Fish and Game. Access areas may be identified by installing a name plaque of the same color below the access directional sign. Sportsman access signing shall be brown reflectorized background with white reflectorized lettering.

SECTION 182.00 – PORT OF ENTRY AND TRIP PERMIT SIGNING

182.01 General. The Idaho Transportation Department is responsible for the installation and maintenance of all signs at and in advance of state Port of Entry (weigh) stations as stated in Title 67, Section 2926, of the Idaho Code.

Special signing requirements for locations that have a port of entry, rest area and interchange at one site or for locations having inadequate space for the required signs shall be reviewed by the Headquarters Traffic Section for the determination of proper signing sequence.

182.02 Permanent Weigh Stations. Port of entry stations required to use permanent signing are those manned by permanent port-of-entry staff and minor satellite or roving facilities used on a frequent basis. Figure [182.02-01](#) illustrates the proper signing sequence for permanent Port of Entry stations along divided highways that may have a Weigh-in-Motion system installed and may coexist with a Rest Area. Figures [182.02-02](#) and [182.02-03](#) illustrate the proper signing sequence for permanent Port of Entry stations along non-divided highways that DO NOT have a Weigh-in-Motion system installed.

All roving POE truck inspection sites used on an infrequent basis should use the latest traffic control plan and portable signs as shown in the POE Procedures Manual.

182.03 Regulatory Message Signs

W/B



R13-1 (L or R) (P)

R13-1A (L or R) (I)

W/B



R13-5 (L or R) (P)

R13-5A (L or R) (I)

Install R13-1 or R13-1A signs in advance of all permanent weigh stations. Install R13-5 or R13-5A signs in advance of an R13-1 sign where a Weigh-in-Motion system is in place.

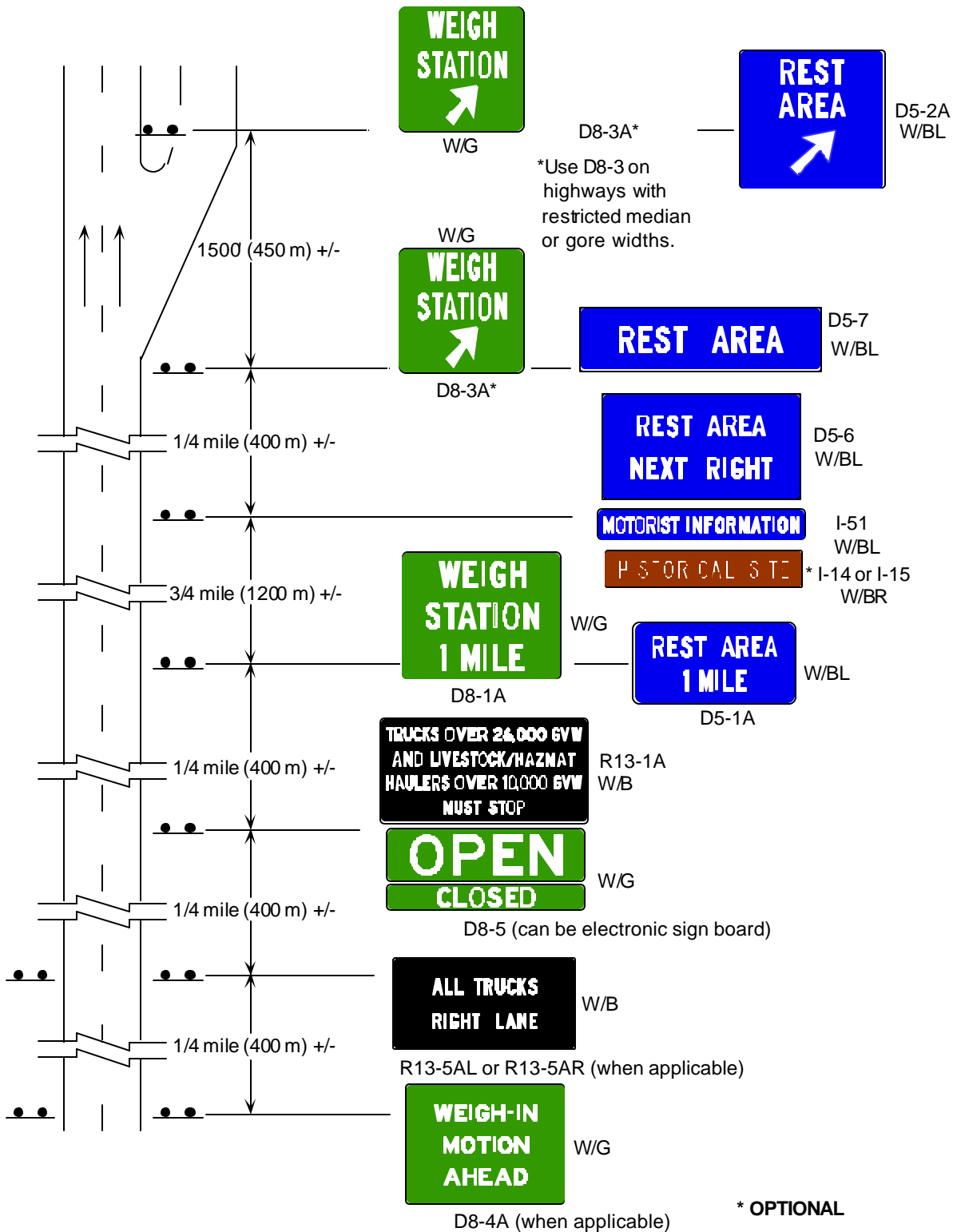


Figure 182.02-01

Signing For Permanent Ports Of Entry (Weigh Stations) And/Or Rest Areas On Interstates And Highways With Four Or More Lanes

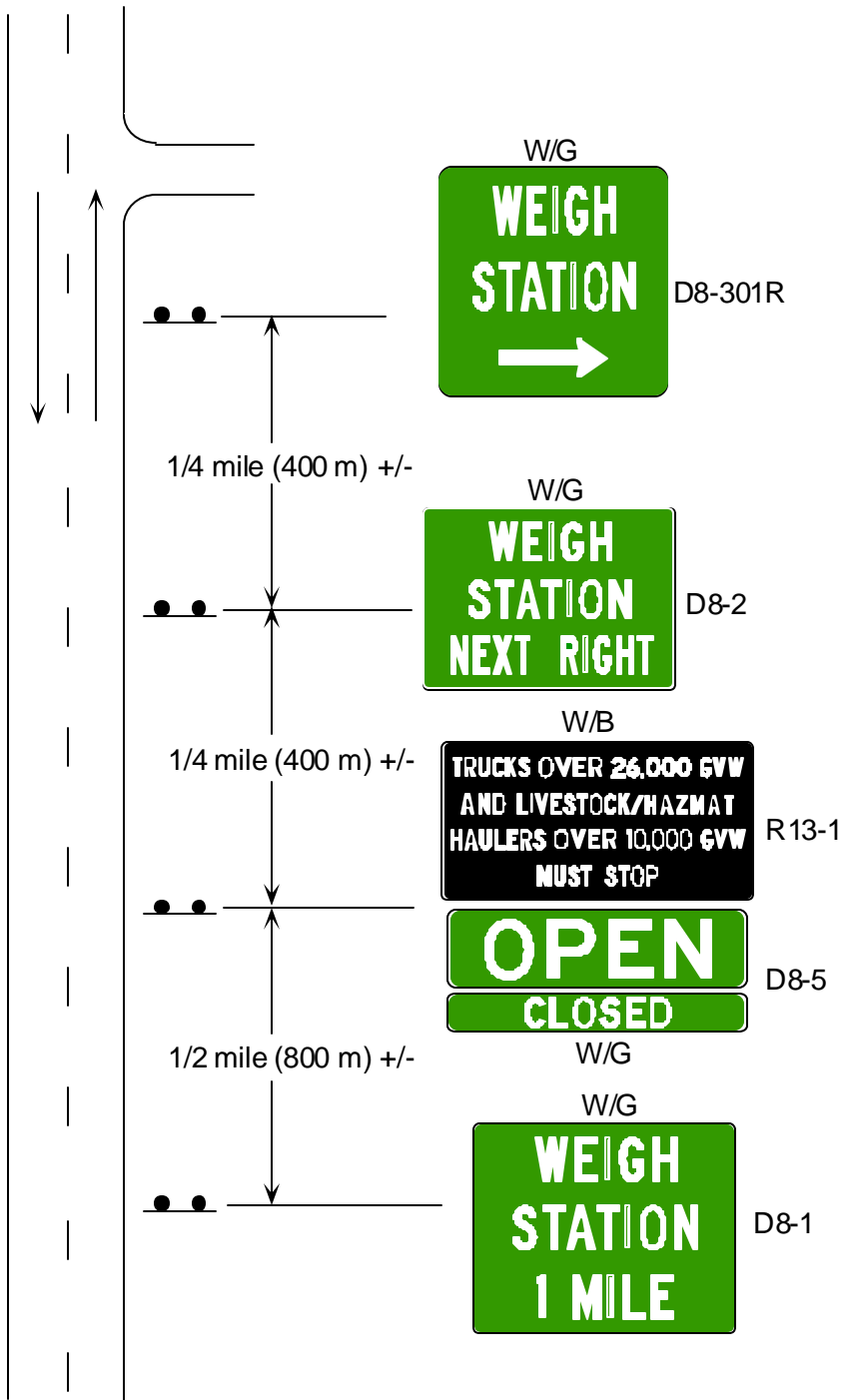


Figure 182.02-02

Typical Advance Signing Sequence For Ports Of Entry (Weigh Stations)
On Non-Divided Highways With Perpendicular Access

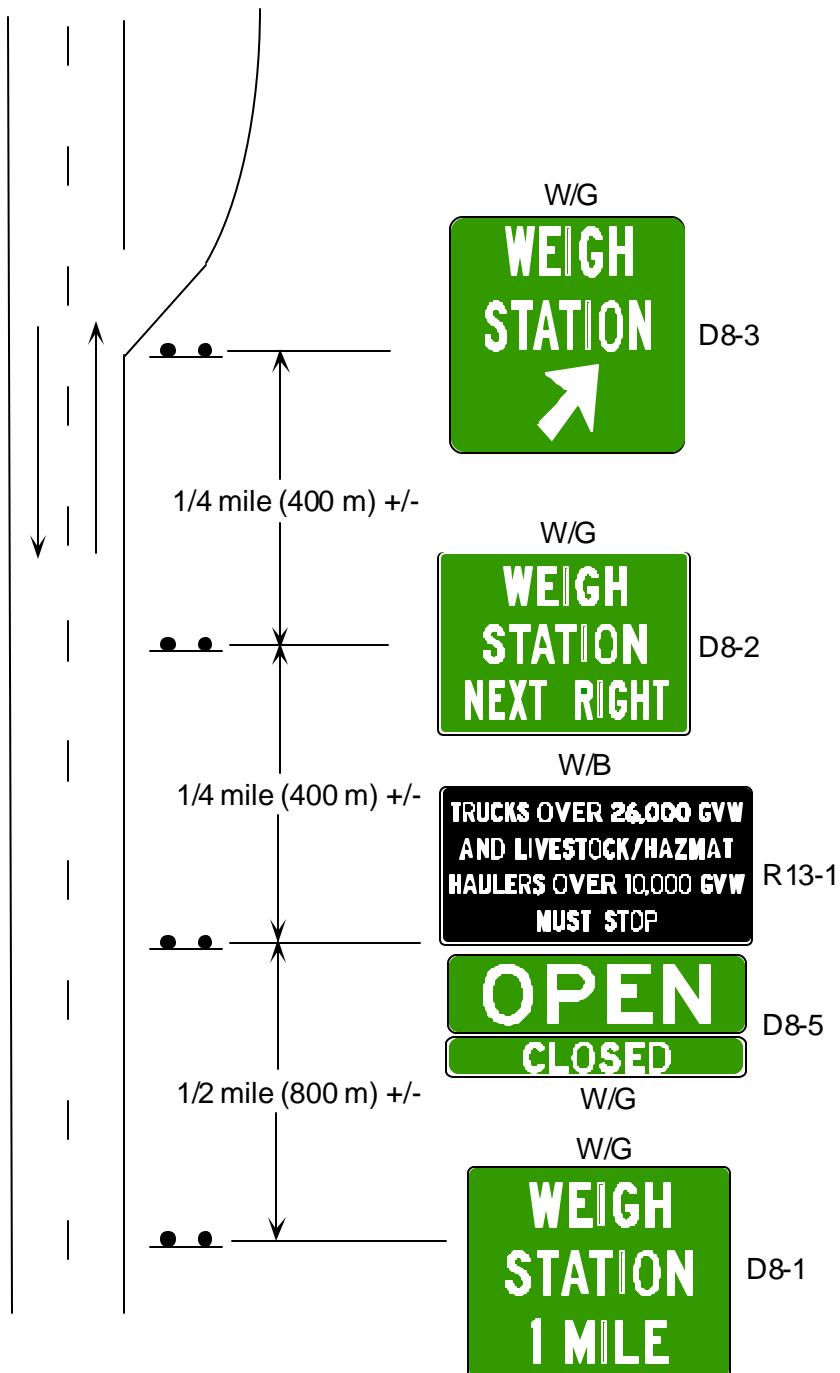
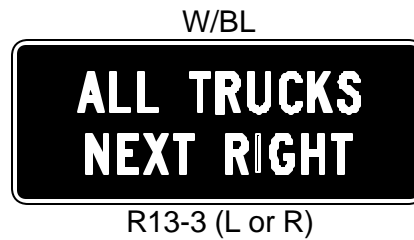


Figure 182.02-03

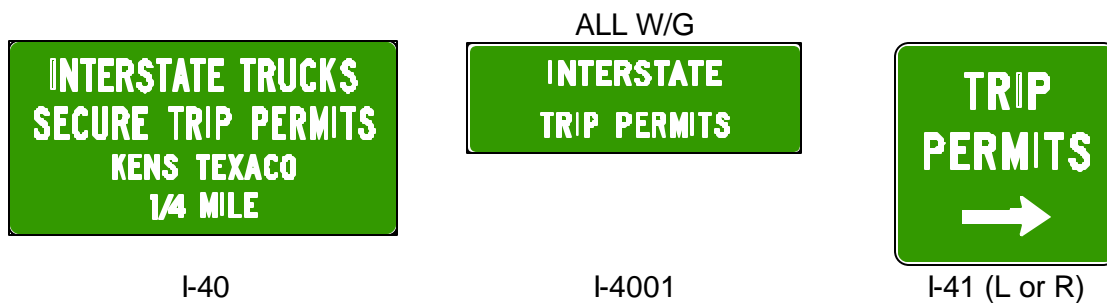
Typical Advance Signing Sequence For Ports Of Entry (Weigh Stations)
On Non-Divided Highways With Turnout Access

182.04 “ALL TRUCKS NEXT RIGHT” Sign



Install the “ALL TRUCKS NEXT RIGHT/LEFT” (R13-3) sign only in advance of Ports of Entry (weigh stations) located on non-divided highways having dedicated deceleration lanes.

182.05 Trip Permit Signs



Install the “TRIP PERMIT” sign for those vendors approved to issue interstate truck trip permits for travel through Idaho.

SECTION 183.00 - SPECIFIC SERVICE SIGNING

183.01 General. There are four areas of motorist specific service signing:

1. Highway Service Signing (Section [179.00](#))
2. Interstate and Primary Off-R/W Directional Signing
3. Controlled Access Motorist Service Signing (Logo Program)
4. Tourist-Oriented Directional Signing (TODS)

NOTE: Local communities should be assured that adequate and proper signing will be provided on alternate routes to encourage travelers to use local businesses and services (see Design Manual Section [400](#))

183.02 Interstate and Primary Off-R/W Directional Signing. This signing is covered by Administrative Rule [39.03.61](#), “Directional and Other Official Signs and Notices,” dated 2/12/90. These signs cover information about public places owned or operated by federal, state, or local government; publicly or privately owned natural phenomena, historic, cultural, scientific, educational, and religious sites; and areas of natural scenic beauty or naturally suited for outdoor recreation deemed to be in the interest of the traveling public and located on private property adjacent to the highway.

Permits must be obtained for each of these signs. The district will prepare these permits and forward to the Right-of-Way Section for approval.

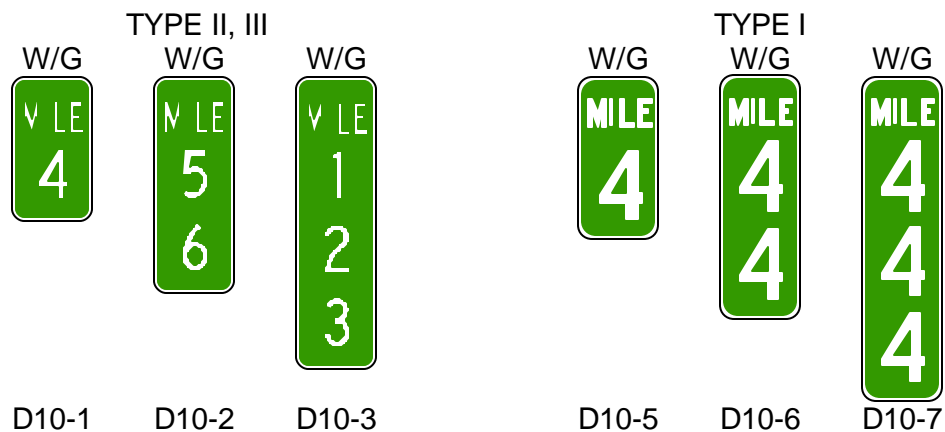
183.03 Controlled Access Motorist Service Signing (Logo). This signing is referred to as the interstate logo program. However, it also applies to other fully controlled access highways. The rules and regulations covering these signs were adopted under Administrative Rule [39.03.62](#), “Logo Signs,” dated 9/17/98, adopting as the reference publication “Signs Giving Specific Information in the Interest of the Traveling Public Along the Interstate Highways and Other Fully Controlled Access Facilities”. The MUTCD, Chapter 2F, Specific Service Signs, also outlines the requirements for this type of signing.

Permits are required for these signs. The district will prepare these permits and forward them to the Traffic Section for approval and processing.

183.04 Tourist-Oriented Directional Signs (TODS). These signs are allowed on the primary and secondary state highway system, excluding fully controlled access segments, to provide directional information to tourist-oriented businesses, seasonal agricultural products and motorist services that are not visible from the highway. They are covered by Administrative Rule [39.03.64](#) adopting as the reference publication “Tourist Oriented Directional Signs (TODS) for Services and Activities Along the State Highway System Except Fully Controlled Access Facilities”, dated 3/17/99. The MUTCD, Chapter 2G, also outlines policies on TODS signing.

Permits are required for these signs. The district will prepare these permits and forward them to the Traffic Section for approval and processing.

SECTION 184.00 - MILEPOSTS



184.01 General. Mileposts should be erected on all state highways to provide a means for identifying a specific reference point on the highway. It is important that mileposts be promptly installed on new additions or changes to the highway system.

The roadway plans should show the location of milepost markers and milepost equation markers with stationing to aid in conversions between mileposts and stationing.

The milepost sequence should run from the south boundary of the state or south termini of the route to the north for a generally north to south route. Conversely, an east to west route would have the mileposts beginning on the west termini.

184.02 Milepost Location. Type I mileposts shall be installed on both sides of expressways and freeways. All other multilane highways should have Type II mileposts installed on both sides. Type III mileposts should be installed on two-lane, two-way highways on the right-hand side of the road in the direction of increasing milepost numbers.

Mileposts may be installed from 4 to 30 feet (1.2 m to 9.1 m) from the shoulder.

The distance from the pavement edge should be consistent for a section of road. A distance just beyond the normal ditch line is desired. However, a distance just behind the guardrail may be required if steep slopes are encountered.

Locate milepost markers at one-mile intervals. Where an intersection, bridges or other interference prevents installing the milepost at its correct location, it may be moved either direction as much as 50 feet (15 m). If it cannot be placed within that degree of accuracy, it should be omitted entirely.

Any milepost temporarily removed must be returned to its original location.

Retain previously established milepost locations when a highway is reconstructed on alignment overlaying an existing route. If the new alignment differs from the highway being replaced, install the milepost markers to continue the milepost sequence of the existing highway. In either case, show the milepost locations by stationing on the plans.

184.03 Milepost Design and Installation. Fabricate and install mileposts in conformance with Standard Drawing I-20 and Idaho Transportation Department Standard Specifications for highway construction. Milepost signs located in line with delineator posts shall have the bottom of the sign at the same height as the delineator.

Mileposts located beyond the normal line of delineators and still inside the clear zone for roadside safety should have the same mounting height as other information signs.

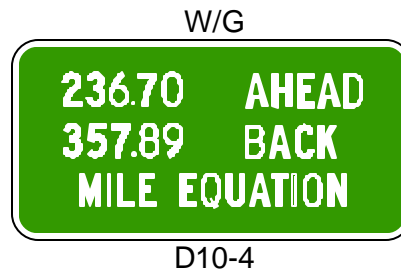
Milepost plates shall have reflectorized green background with white reflectorized letters (MILE) and border. The numbers should have white class "B" high intensity reflectorized sheeting.

All mileposts should have concrete foundations or be grouted into a hole in solid rock.

184.04 Milepost Log. The Transportation Planning Division maintains a master file of the Milepost and Coded Segment System (MACS) which can be accessed by individuals through the mainframe computer system to obtain the milepost log. on the milepost log should be submitted on an ITD-2185 (Figure [184.04-01](#)), Milepost Log Worksheet, to the nearest 0.001 mile. Log intersections at 0.001 mile. Submit requests for other changes on ITD-2184 (Figure [184.04-02](#)), Request for Milepost Changes.

The Transportation Planning Division also maintains master records of the video log. Planning will provide updated video logs as they become available. As new projects are district shall prepare a milepost log worksheet on form ITD-2185 within a month after a project on new alignment is completed and submit it to the Transportation Planning Division to have the new section video logged.

184.05 Milepost Equation Markers



When the mileage of the project differs from that of the old route, a milepost equation will be necessary at the high milepost end of the project. Show it on the plans and install it at the first point that the new construction returns to the old alignment and remains there. Milepost equation markers shall be installed in all cases where a milepost equation is established.

Mount these milepost equation markers at the exact point of equation. They may be mounted at the same lateral position from the roadway as a normal milepost, although it is more desirable that they be mounted further from the roadway in a position clearly visible from the roadway.



Project Number	
----------------	--

[illegible]

Figure 184.04-01 Milepost Log Worksheet (ITD-2185)

Proposed Milepost Changes



See Design Manual Sections 400 and 800; See Traffic Manual Section 184

See page 2 for Instructions

To Transportation Planning Section		Date	
From		By	
Project Number	Key	Location (Attach Vicinity Map)	Proposed Const. Yr.

Reason for Change

<input type="checkbox"/> New Route	Route	
<input type="checkbox"/> Relocation	Milepost	to
<input type="checkbox"/> Reconstruction/Realignment	Segment Code	
<input type="checkbox"/>	Milepost	to
	Segment Code	

Note: If project traverses multiple segment codes, list Milepost and segment codes for each.

Description of Changes

Approved	Yes	No	Date	Signature
District __ Engineer	<input type="checkbox"/>	<input type="checkbox"/>		
Traffic Engineer	<input type="checkbox"/>	<input type="checkbox"/>		
Transportation Planning Mgr.	<input type="checkbox"/>	<input type="checkbox"/>		

SECTION 185.00 - GOVERNMENT BOUNDARY SIGNS

185.01 General. Government boundary signs mark boundaries for a state, county, Indian reservation, city, and village. These boundary locations usually denote areas of law enforcement and legal jurisdiction so it is important that they be defined. Additionally, they provide the motorist with location information if he is unfamiliar with an area.

185.02 Idaho Entrance Sign



I-2

The Idaho entrance signs (I-2 on conventional highways and I-3 on interstate highways) shall be installed on all state highways where they enter Idaho. It is not necessary to install this sign exactly at the state boundary since other regulatory and warning signs are sometimes more important and should take precedence.

185.03 Entering Community Signs



I-401 (I)



I-4 (P)



I-6

Where appropriate, the Department will install and maintain a standard boundary sign at the corporate boundaries of incorporated cities and villages. Placement of corporate boundary signs on the state highway system by local jurisdictions shall be limited to locations where jurisdictional boundaries are contiguous and perpendicular to the highway centerline.

The city populations shown on this sign shall conform to the latest official census of the community as provided by the Idaho Department of Commerce website www.idoc.state.id.us/census/20XX. The Districts shall initiate sign updates after the release of federal census data and at intermediate intervals when requested by a community when the Idaho Department of Commerce reports indicate that population figures have increased or decreased by at least five (5%) percent.

Install the I-6 sign for unincorporated communities through which the highway passes near the beginning of the development adjacent to the highway.

A customized city identification sign(s) may be used on ITD's right-of-way if it is furnished, placed, and maintained by the city requesting such sign(s) only after the design of the sign and its location have been approved by the District Engineer. Unincorporated communities are not to be signed with customized signs.

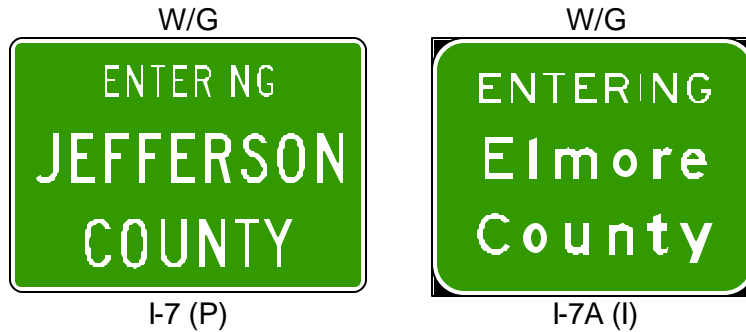
If a customized sign is not located at or near a city boundary, a standard boundary sign (I-4) may be placed at the boundary. Customized signs will not be permitted on the interstate highway.

A city customized sign shall meet the following criteria:

- The design of the signs shall be in good taste and devoid of any advertising. Animation, flashing lights, or other devices to attract attention shall not be permitted. If illuminated, lights shall not cause a glare nor be directed into the face of approaching traffic.
- In addition to the city name the signs may contain the legend "Welcome To", and/or city logo. The name(s) of the local official(s) shall not be permitted on these signs. Any other message, such as a slogan, shall be brief and devoid of any commercial name or sponsoring organization name. Variable messages such as announcements of coming events are not permitted. Logos, if used, shall be devoid of any advertising and shall not resemble official traffic control devices.
- Customized signs replacing standard boundary signs shall have a retroreflectorized white legend on a retroreflective green rectangular background.
- The sign(s) shall be placed at or within the city boundary. Installations that would create a sight distance problem or traffic hazard will not be allowed.
- The location of an existing traffic control device or the need for a new device shall take precedence over the location of a city boundary/ identification sign, either existing or proposed.
- Relocation and/or removal of sign(s) as a result of highway improvement projects, such as roadway widening, shall be the responsibility of the city. Signs not relocated by the city shall be removed by the Department and the city billed accordingly.
- Signs replacing standard boundary signs shall be placed in the same/approximate location on the corporate limits. Customized signs not at corporate limits should be placed near the highway right-of-way line. The signs shall not be located in the median. Materials for the signs shall meet Department specifications. Signs shall be retroreflectorized with minimum height of letters of 6 inches (150 mm). Sign supports shall be of approved breakaway design unless protected by guardrail.
- A permit to use the right-of-way must be obtained from the District Engineer before customized signs may be installed.
- The area of the sign shall be limited to 32 square feet (3 m²).

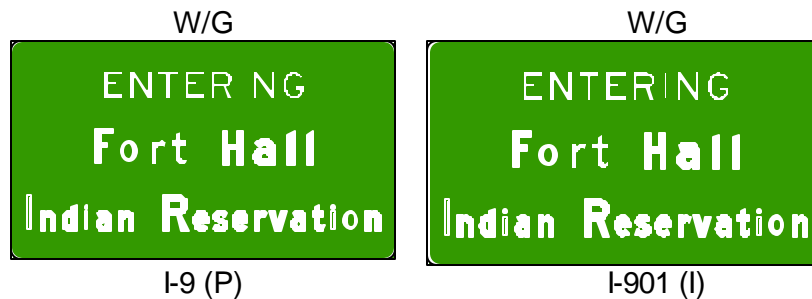
- Signs must be maintained in good condition. Otherwise, the signs will be subject to removal by the Department.

185.04 Entering County Signs



Install the “ENTERING County” signs for each direction of travel at all county lines on the state highway system.

185.05 Indian Reservation Boundary Signs



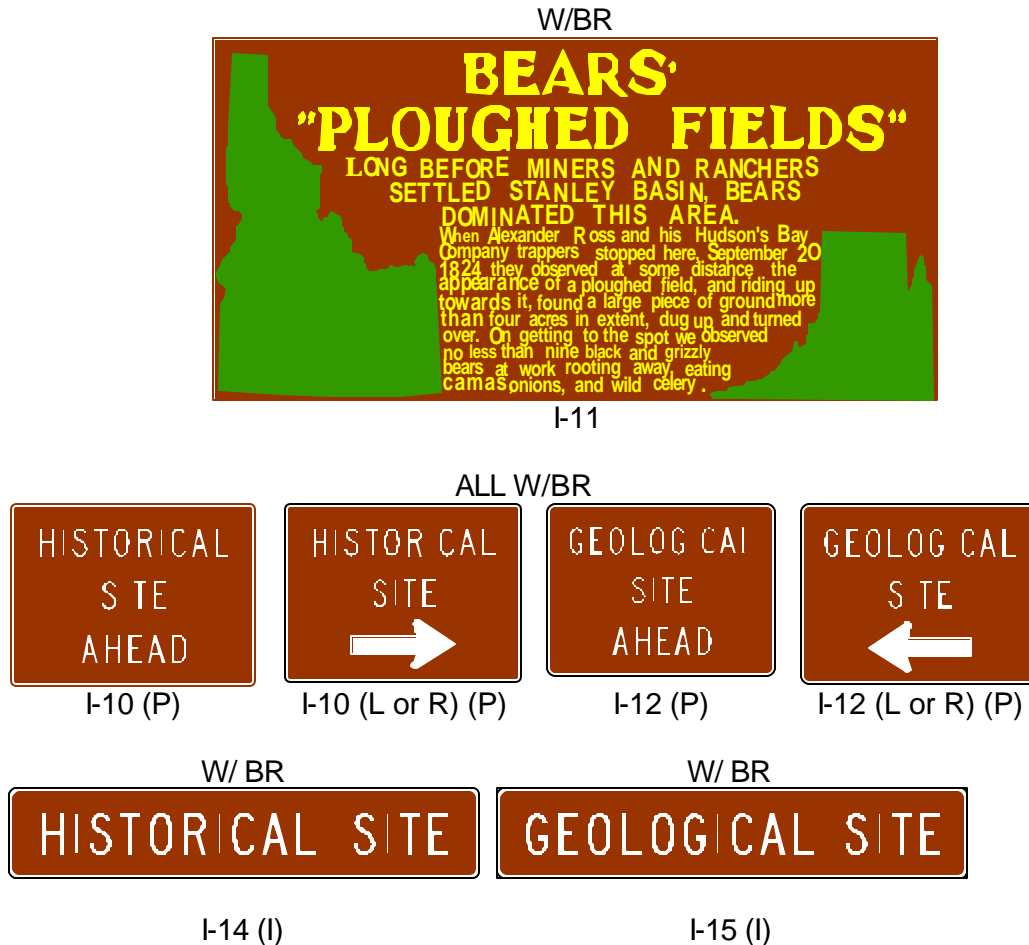
Install the “ENTERING/LEAVING Indian Reservation” boundary signs at all locations on the state highway system that enter or leave an Indian Reservation since special legal jurisdiction requirements apply within the boundaries.

185.06 National Forest Boundary Sign



The “ENTERING/LEAVING National Forest” boundary signs should be installed at all locations on the state highway system that enter or leave a national forest. These signs are not required at those locations where the Forest Service has erected large entering signs by special permit.

SECTION 186.00 – HISTORICAL, GEOLOGICAL AND MUSEUM SIGNS



186.01 New Installations. The Idaho Transportation Department fabricates and installs all historical, geological and museum signs pertaining to locations on the state highway system. Each historical and geological site sign is numbered so that it can be referenced on maps, tourist pamphlets, or more detailed supplemental brochures. The motorists shall be advised of these signs by appropriate advance signing.

An historic district is property officially designated by the State Historic Preservation Officer and approved by the State Architectural Historian. Historic attractions will be considered for signing if they are currently listed in the National Register of Historic Places or the State Inventory of Historic places.

All proposed historical and geological signs and legends shall first be submitted to the District Traffic Engineer to determine a location for a sign placement. Historical and geological panels require placement within a turnout. The proposal will then be sent to the State Traffic Engineer who will present it to the director of the Idaho Historical Society in conformance with Title 67,

Section 4117, Idaho Code, for approval. This will assure accurate data concerning historical and geological locations. Upon approval, the State Traffic Engineer will advise the District and submit a Highway Sign Order Form (ITD-1671) to the Sign Shop for fabrication.

186.01.01 Cultural/Historical Museums. The following criteria apply to signing for cultural/historical museums:

- Museums must be publicly owned and endorsed by the Idaho State Historical society.
- The museum must be located within 15 miles (24 km) of the interchange or intersection. See Section 176.06, Interstate Supplemental Guide Signs, for interstate application.
- The principal source of visitors must be motorists not residing in the immediate vicinity.
- The museum must be open at least 7 hours per day, 5 days a week, including Saturday and/or Sunday during its normal operating season.
- Adequate parking must be provided and on-premise signing must be readily visible.
- Signs will only be installed and maintained on the state highway system for museums located outside city limits and not visible from the highway.
- For signing on interstate highways, the requirements for supplemental guide signs listed in Section [176.06](#) shall be met.

186.02 Existing Markers. Any organization having an existing historical or geological marker located immediately adjacent to the highway and desiring the Department to sign and maintain it, shall first deed the property and sufficient surrounding parking area to the Idaho Transportation Department. The sign legend must also be approved by the Idaho Historical Society. Advanced signing shall be installed for the markers.

186.03 Turnouts. Locate all historical and geological signs and markers at turnouts having adequate parking areas off the highway traveled way. If an existing turnout is not available, then an adequate turnout area must be constructed before the signing is installed.

Historical, geological and other scenic points of interest signs are not permitted on the interstate highway system except at rest area locations.

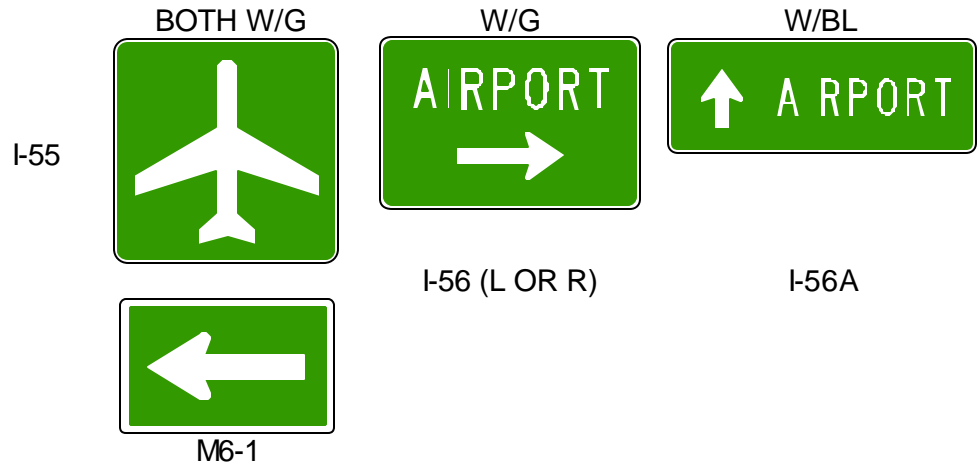
186.04 Locations Off the State Highway System. The Department may sign for historical or geological sites located off or not visible from a state highway under the following conditions:

- The director of the Idaho Historical Society declares the site to be of major historical or geological importance and requests in writing that the Idaho Transportation Department install an appropriate sign at the access road connection to the state highway.
- An agreement is completed between the Idaho Transportation Department and the governmental agency having jurisdiction over the access road to the site providing that the road will be adequately maintained and signing installed directing tourists to and from the site location. This shall be done before the marker is fabricated.

- In addition to the usual historical and geological data, the sign legend shall include descriptive information and/or a map directing tourists to the site and back to the highway or starting point.

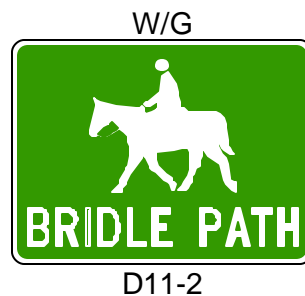
SECTION 187.00 - GENERAL INFORMATION SIGNS

187.01 Airport Directional Signs



Use the "Airport" symbol (I-55) sign for signing commercial airports. The legend (I-56) sign may be utilized for general aviation airports.

187.02 Bridle Path Sign



The "BRIDLE PATH" (D11-2) sign may be installed where a section within the right-of-way has been established exclusively for equestrians.

187.03 City Center Signs



The “CITY CENTER” (I-20) sign may be used where the highway is adjacent to or within an incorporated city limits, but does not penetrate the city center. If a highway junction exists adjacent to a city limits and the city name appears on the destination board, the city center sign is not required.

The “CITY CENTER” (I-21) sign will include the city name only when the highway bypasses the corporate limits and community entrance signs are not installed.

Install the auxiliary sign “VISITOR SERVICES” (I-22) in conjunction with the city center signing when the normal business development is located at the city center. However, do not use the sign when the development is adjacent to the highway and provides all of the normal services required by the motorist.

187.04 Physical Feature Signs



The “HILL” (I-23) and “SUMMIT” (I-24) signs may be installed at the crest or the bottom of hills and summits, where the motorist might be interested in both the hill or summit name and elevation of the roadway.

The elevation sign (I-25) may be installed at the crest of points of interests such as the top of dams, viewpoints, etc.

Install the “RIVER” (I-28), “CREEK” (I-26), or “CANAL” (I-27) signs where the highway crosses rivers, creeks, or canals that may be of interest to the passing motorist. Use the larger legend for the more important rivers and streams. The sign should be omitted at obvious locations where the river, creek, or canal is generally parallel and adjacent and crosses the highway frequently. Sign only the first crossing and minimum confirming locations.

187.05 Litter Signs



It is intended to use the "Idaho is too great to litter" sign (I-2901) at all state highway entrances. Other state line signing takes precedence over this sign and accordingly the sign can be installed at some distance within the state. The sign can also be installed on major recreation routes with some intermittent use on long sections where necessary to provide good coverage.

Install the "Anti-Littering" (R16-14) sign at one-mile intervals on those sections where littering is a major problem. Keep to a minimum since a proliferation of these signs would cause an adverse impact on the highway environment.

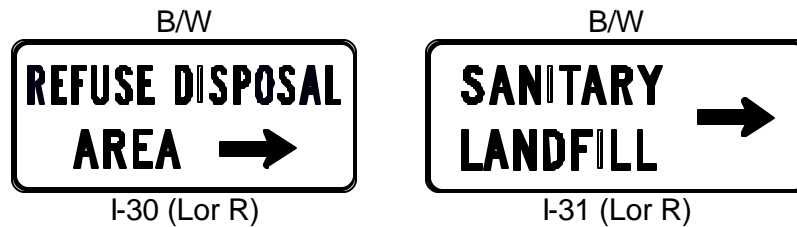
187.05.01 Adopt-A-Highway Signing



The Department has implemented an adopt-a-highway program where private groups are authorized by agreements administered in each district to perform litter cleanup services on adopted sections of highway. As a condition of the program, the Department agrees to install signs at each end of the adopted highway segment identifying the group by name or acronym. Group name or acronym should not include any commercial advertising.

Appropriate signs should be ordered and installed after an adopt-a-highway agreement has been approved by the district.

187.06 Refuse Disposal Signs



The “Refuse Disposal Area” (I-30) or “Sanitary Landfill” (I-31) signs may be installed to properly identify refuse disposal areas. Coordinate the request for these signs from local agencies through the local state health department representative, to verify the proper sign legend.

187.07 “WATER UNSAFE TO DRINK” Sign



Install the “WATER UNSAFE TO DRINK” (I-32) sign in areas where contaminated water is accessible to motorists. Locate the sign as close as possible to the area where an individual would attempt to drink the water.

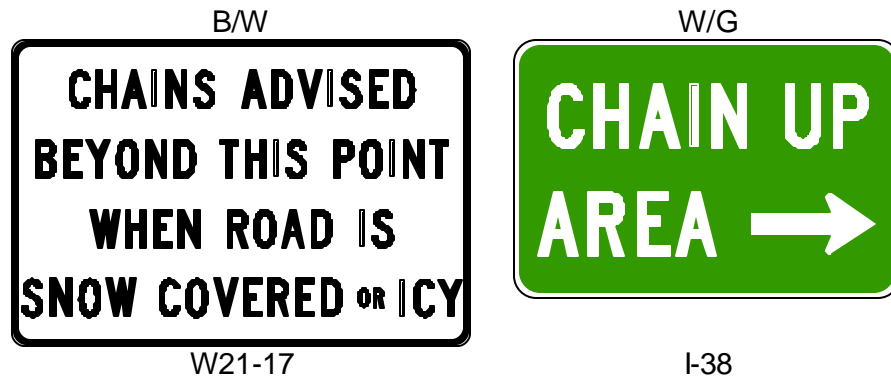
187.08 “VISITOR INFORMATION” Signs



The Idaho Transportation Department may install “VISITOR INFORMATION” signs at information centers when such information centers meet the following requirements:

- It shall be of a noncommercial nature.
- It shall be located near the highway.
- It shall be located where such activity does not create congestion or constitute a traffic hazard.

187.09 Chain Up Signs

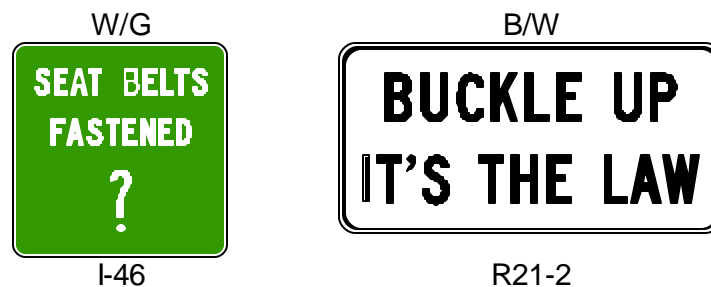


Install "CHAINS ADVISED BEYOND THIS POINT" (W21-17) signs on roads in mountainous areas and on long grades where winter road conditions are hazardous.

The "CHAIN UP AREA" (I-38) sign should be installed in advance of a turnout that has been provided for motorists to mount chains.

Hinge or remove all of the above signs during seasons when snow and ice are not expected.

187.10 Seat Belt Signs



Install the "SEAT BELTS FASTENED?" (I-46) sign, or other approved seat belt informational sign, at the exit of all major state highway and law enforcement properties. The Office of Highway Safety coordinates seat belt campaigns.

Install the "BUCKLE UP IT'S THE LAW" (R21-2) sign at Idaho entrances, rest areas, ports of entry and other areas requiring emphasis.

187.11 Historic Trail Signs

ALL W/BR



I-6301



I-63



I-59



I-65



I-66

ALL MULTI-COLORED



I-59



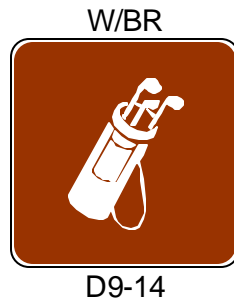
I-69



I-70

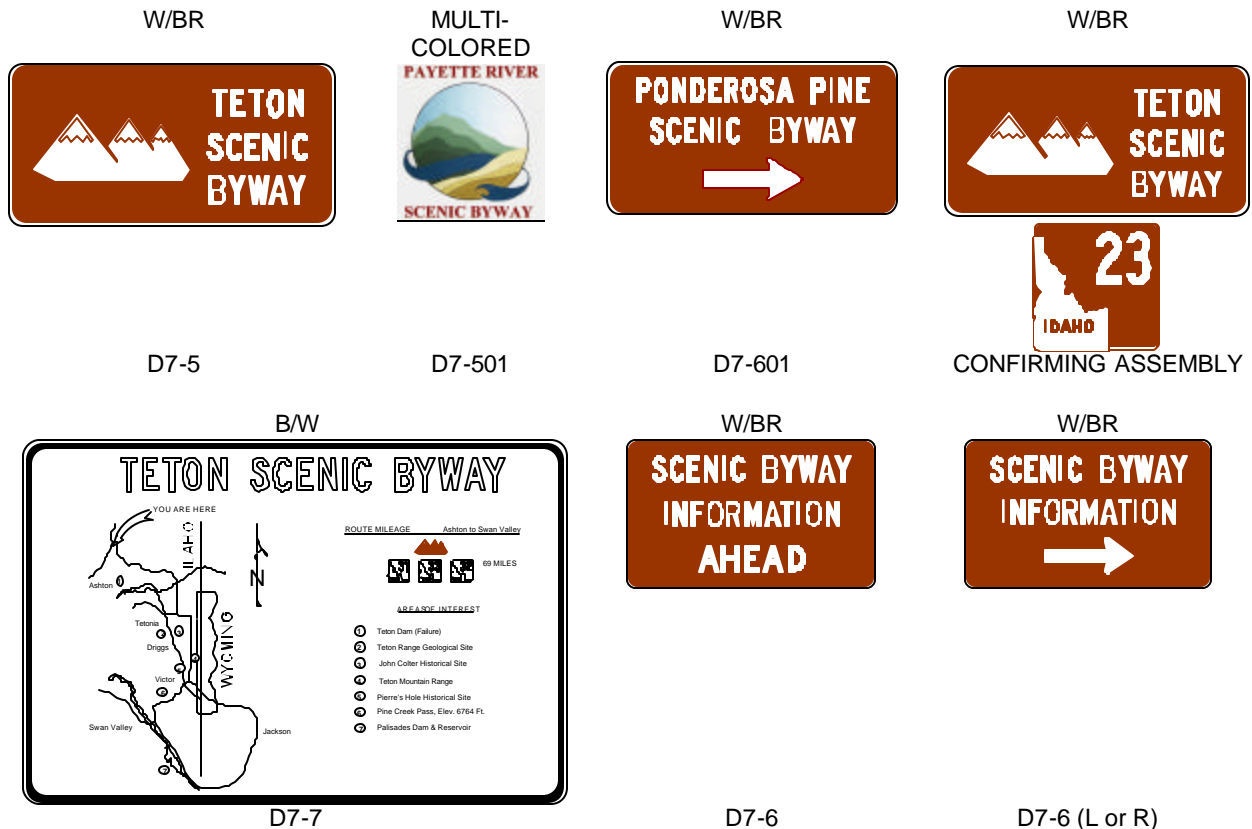
National historic trail signs may be installed at all locations where the original trail intersects or coincides with the state highway system. “AUTO TOUR ROUTE” signs may be installed only along federally-approved auto tour routes. Federal approval is determined by the National Trail Coordinator of the National Park Service. Although not a federally-recognized historical route, signing for Goodale’s Cutoff will be allowed. “OREGON TRAIL” (I-59) and “GOODALE’S CUTOFF” (I-65) shall not be used for any other historical route.

187.12 Golf Course Signs



Golf Course symbol signs may be installed for public golf courses adjacent to but not visible from the highway. These signs are not intended for use on the interstate highway system. Golf courses which are located a significant distance (over 5 mi or 8 km) from the highway will not be signed. Confirming signs should be installed on the approach roadway.

187.13 Scenic Byway Signs



The Idaho Transportation Board in December 1976 authorized “designation of scenic routes in Idaho utilizing route markers and maps.” In 1991 these routes were revised to “scenic byways” conforming to AASHTO Guidelines on designation of scenic highways. It is the Board’s intent that the scenic designation be limited to routes of unquestionable scenic quality.

The Board further directed the Idaho Transportation Department to work through the area-wide planning organizations around the state to obtain preliminary recommendations as to the appropriate scenic and historic routes for state highways and local systems. The Division of Planning submits requests for scenic byway and historic route designations to the Board for approval. Districts shall submit any scenic byway and historic route requests to the Division of Planning.

An exception to the MUTCD by the Department allows multi-colored versions of the scenic byway and historic route signs (D7-501). Signing shall be administered by the Headquarters Traffic Section, and all signs shall conform to the “Standards and Procedures for Multi-Colored Byway Signs on the State or Local Highway System”.

All scenic byways shall be signed at both ends with a “SCENIC BYWAY INFORMATION” (D7-7) sign installed within a turnout where the motorist can safely pull off the highway to read the sign. (See Figure [187.13-01](#)) The confirming route marker shall be installed at both ends of the scenic byway or historic route and past every community and other state highway junctions. Provide directional guidance from a state highway to a scenic byway with a D7-601L/R sign.

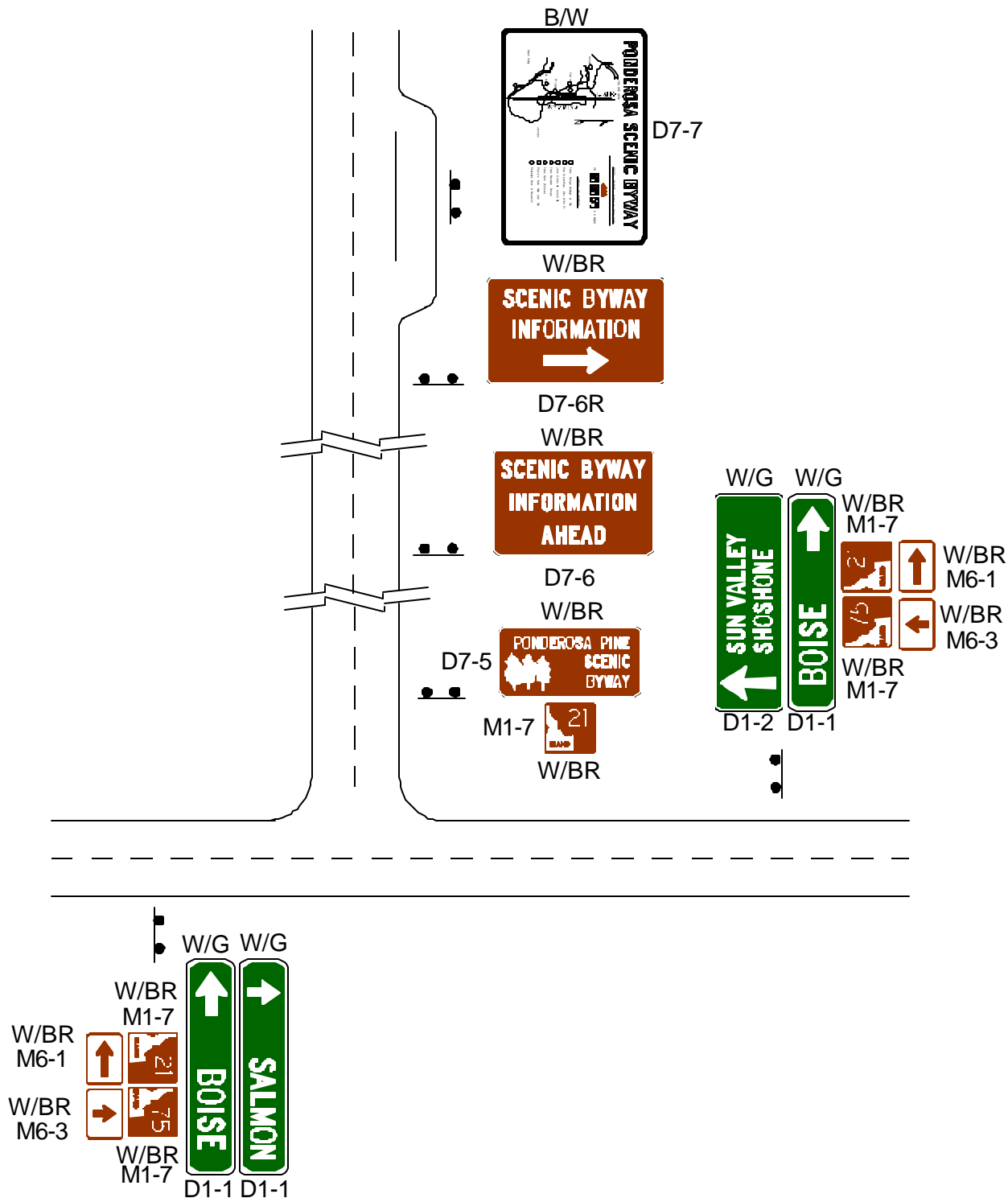
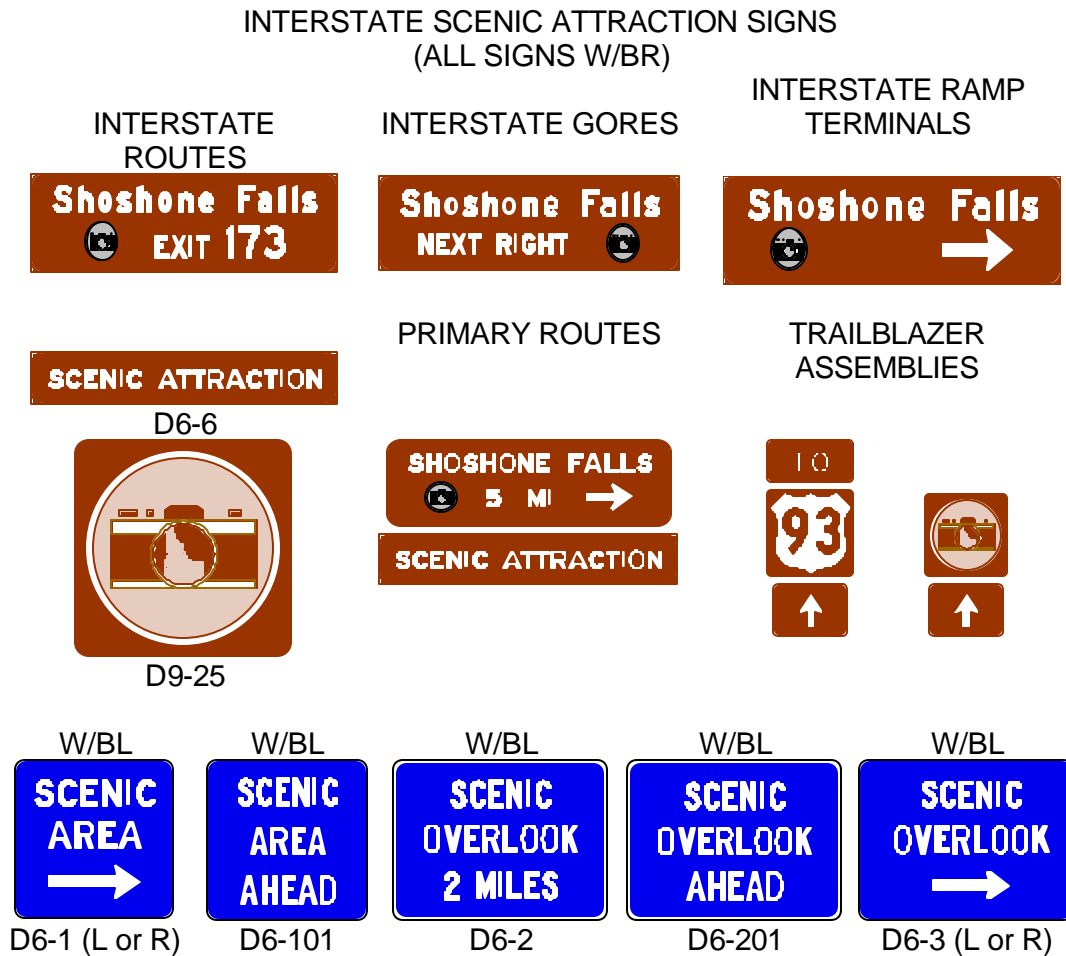


Figure 187.13-01 Scenic Byway Signing

187.14 Scenic Attraction Signing (I.C. 67- 4720)



Procedures for Signing Scenic Attractions:

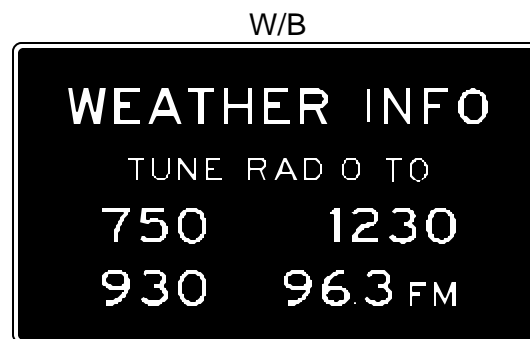
- Before signing will be provided to an attraction, assurance must be obtained from the local government or operator of the attraction, as applicable, that follow-through signing consistent with the MUTCD will be provided.
- Signing will not be provided if the attraction is readily visible and has direct access to the highway.
- Signing will normally not be provided in urban areas or within City limits. Signing for attractions within city limits is the responsibility of the local government and the operation of the attraction. Signing for qualified city attractions can be signed on the state highway within the city limits by revising the city's existing Cooperative Agreement for Maintenance of State Highway ([ITD-2392](#)) and erecting and maintaining the appropriate follow-through signing.
- The attraction must be located within 25 miles (40 km) of the interchange or intersection being signed. See Section [176.06](#), Interstate Supplemental Guide Signs.
- For attractions located more than one mile (1.5 km) from the interchange or intersection, mileage information may be shown on the ramp terminal or direction signs.

- Signing will typically be provided from the state highway nearest to the attraction. More than one highway may be signed.
- All attractions must be open to all segments of the motoring public, without appointment, at least seven hours a day, five days a week including Saturday and/or Sunday during its normal operating season. Historical and cultural attractions that have features within a building or a restricted outdoor area shall include available restroom facilities and drinking waters licensing where required and adequate parking accommodations. Undeveloped historic and cultural features not located within buildings or a restricted outdoor area shall include adequate parking accommodations and an informational device to provide public knowledge of the features.
- Signs must be removed or covered for seasonal closures.
- Attractions must be served by a two-lane, all-weather road, provide adequate parking accommodations and have a turn-around area.
- The attraction must be maintained in good repair and presented in a professional manner.

187.15 Radio Information Signs

187.15.01 CB Radio Channel Monitoring. Since CB Channel 9 is accepted as the established emergency channel, signs will not be posted to indicate that the channel is monitored by a particular agency. A CB radio operator may assume that there will be some response expected on Channel 9.

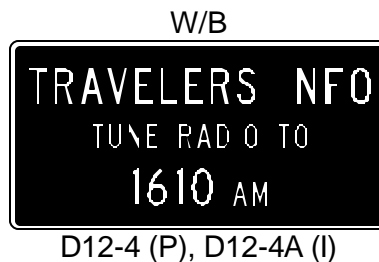
187.15.02 AM/FM Traveler Information



- Radio-Weather Information (D12-1) signs may be used on rural highways where weather commonly creates an undue hazard.
- Only the numerical indication of the radio frequency shall be used to identify a station broadcasting weather travel information.
- A maximum of four frequencies may be shown on each sign.
- The radio station should have a signal strength to adequately serve 40 miles (65 km) along the roadway.

- Signs should be spaced according to needs, but ordinarily not closer than 20 miles (32 km) apart for each direction of travel.
- A particular radio frequency may be shown a maximum of two times in one direction along the main line.
- Only radio stations whose signal will be of value to the traveler and who agree to carry the two items below are to be identified on weather information signs:
 - o Periodic weather warnings at least every 15 minutes during periods of adverse weather.
 - o Road condition information concerning the roadway being traveled once every half an hour when required, supplied by an official agency having jurisdiction.
- The stations to be included on the signs should be selected to represent major broadcasting stations in the area to provide (1) maximum coverage to all motorists on AM and/or FM frequencies, and (2) with preference for 24 hours a day, 7 days a week broadcast capability.
- When used, the appropriate sign message is Weather Info/Tune Radio to/ (frequency).

187.15.03 Highway Advisory Radio (HAR)

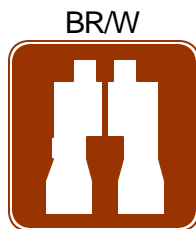


Travelers' Information Station (TIS) or HAR is a special class of low-power AM radio broadcasting on the reserved frequencies of 530 kHz or 1610 kHz. Signs (D12-4) may be installed to identify the numerical frequency only.

- Station must be operated by an official government agency or a government-affiliated agency. If a government agency chooses to designate another agency, such as a Chamber of Commerce, as its official station operator, such designation must be made in writing and approved by the State Traffic Engineer.
- The operating agency must submit all requests for signs for such stations in writing to the District Engineer. The District Engineer will forward the request with a recommendation for approval or denial to the State Traffic Engineer. Upon approval, the ITD Sign Shop will fabricate the signs and notify the district of the costs of materials and fabrication. The district will add the materials costs of posts and foundations and bill the operating agency. The signs will be installed by the district at ITD expense.
- Broadcast must be a minimum of 12 hours a day, beginning no later than 7:00 a.m., and 7 days a week.

- Suitable information for broadcast includes traffic and road conditions, traffic hazard and travel advisories, directions, availability of lodging, rest stops and service stations, and descriptions of local, state, or national points of interest.
- It is not permissible to identify the commercial name of any business establishment whose service may be available within or outside the coverage area of a TIS.
- Replay of standard broadcast programming or music are not permitted to be broadcast.
- Broadcast material must be of sufficient length as to not be unduly repetitious. The message should be of such length that a motorist can hear at least two repetitions while passing through the zone.
- When used, an appropriate sign message is “TRAVELERS INFO/TUNE RADIO TO/(frequency).”

187.16 Viewing Signs



D9-24

Place the Binoculars symbol (D9-24) on existing wildlife viewing guide signs or as a supplemental sign where applicable. This symbol can also be used as a trailblazer logo. Viewing areas can be located on federal, state or private lands. Verify that all viewing areas have reliable wildlife viewing opportunities and minimal conflicts with other area users. If the viewing area is a business on private property, signing must be considered under the Tourist Oriented Directional Sign (TODS) program (see Section [183.04](#)).

Signing will not be provided along the interstate highways or other fully controlled access facilities. Signing will not be provided at locations where traffic hazards or sign congestion exist, especially where there are no safe viewing turnouts. Signing will normally not be provided within urban areas or city limits. Signing within city limits is the responsibility of the local government.

Before signing will be provided to any viewing area, assurance must be obtained from the local highway jurisdiction that trailblazer signing from the state highway to the site will be provided and that all signing will be consistent with MUTCD requirements. Trailblazer signs shall be in place prior to installation of directional signs along state highways.

187.17 “GEM Community” Signs

BR/W



I-67

The Idaho Department of Commerce will advise the Headquarters Traffic Section when a community has been certified under the Gem Community program. A maximum of four I-67 signs (one sign each direction by route) will be provided for placement on highways accessing the community or county. Headquarters Traffic will process sign orders for new communities entering the program. It will be the responsibility of the community to arrange for sign installation on off-system highways.